



Revisions

APPLICATION FOR SPECIAL PERMIT

Complete, notarize, and forward thirteen (13) hard copies and (1) electronic copy in PDF format to Clerk of the Zoning Board with a **\$1,000.00 Public Hearing Fee** and the required application filing fee (see Fee Schedule below), payable to the City of Stamford.

NOTE: Cost of required advertisements are payable by the Applicant and performance of required mailing to surrounding property owners is the sole responsibility of the applicant. **LAND RECORDS RECORDING FEE \$60.00** for First page - \$5.00 for each additional page)

Fee Schedule

Special Permit 20,000 sq. ft. or less	\$460.00
Special Permit more than 20,000 sq. ft.	\$460.00 + \$30 per 1,000 sq. ft. or portion thereof in excess of 20,000 sq. ft.

APPLICANT NAME (S): DARIUSZ LESNIEWSKI
 APPLICANT ADDRESS: 35 DICKINSON RS DARIEN CT 06820
 APPLICANT PHONE #: 203-858-2514
 IS APPLICANT AN OWNER OF PROPERTY IN THE CITY OF STAMFORD? YES
 LOCATION OF PROPERTY IN STAMFORD OWNED BY APPLICANT (S): 16 REMINGTON ST

ADDRESS OF SUBJECT PROPERTY: 16 REMINGTON ST

PRESENT ZONING DISTRICT: RMF

TITLE OF SITE PLANS & ARCHITECTURAL PLANS: IMPROVEMENT LOCATION MAP

PREPARED FOR DARIUSZ LESNIEWSKI

ARCHITECTURAL PLANS: DARIUSZ LESNIEWSKI RECONSTRUCTION OF EXISTING DWELING.

REQUESTED SPECIAL PERMIT: (Attach written statement describing request)
NEXT PAGE

LOCATION: (Give boundaries of land affected, distance from nearest intersecting streets, lot depths and Town Clerk's Block Number)
N 52° 24' 30" W / N 35° 34' 29" E / S 51° 28' 56" E / S 35° 33' 22" W
PACYK ST IS NEAREST INTERSECTING STREET APPROX 300 FT TOWN CLERK BLOCK # 96
LOTS DEEP 50' x 10'

NAME AND ADDRESS OF OWNERS OF ALL PROPERTY INVOLVED IN REQUEST:

<u>DARIUSZ LESNIEWSKI</u>	<u>16 REMINGTON ST</u>
<u>35 DICKINSON RS</u>	
<u>DARIEN CT 06820</u>	

DOES ANY PORTION OF THE PREMISES AFFECTED BY THIS APPLICATION LIE WITHIN 500 FEET OF THE BORDER LINE WITH GREENWICH, DARIEN OR NEW CANAAN? NO (If yes, notification must be sent to Town Clerk of neighboring community by registered mail within 7 days of receipt of application - PA 87-307).

DOES THE PROJECT RESULT IN THE CREATION OF 10 OR MORE UNITS OR 10,000 SF OR MORE IN FLOOR AREA OR DISTURBANCE OF 20,000 SF OR MORE IN LAND AREA, THROUGH NEW DEVELOPMENT, RECONSTRUCTION, ENLARGEMENT OR SUBSTANTIAL ALTERATIONS? _____ (If yes, then complete the Stamford Sustainability Scorecard per Section 15.F).

The special permits sought are as follows:

- The R-MF zoning allows 30% building coverage (1573 SF). Section 7.3 allows 25% increase, allowing for a maximum of 1,966 S.F. Our proposal calls for 1,820 S.F.
- Density increase 50%
- The R-MF zoning allows min 30' rear setback. Section 7.3 allows 50% decrease, allowing for a minimum 15' from the rear line. Our proposal calls for 23'
- Special Permit under Section 7.3 allows required parking space of one (1) space per dwelling unit.
- Special Permit request to reduce the Light and Air requirement by 50%. The current requirement is 20 ft. After reduction to 10ft, the Applicant can provide 8' on their property, given that the neighboring property is subject to the same regulation.



DATED AT STAMFORD, CONNECTICUT, THIS 27 DAY OF MAY 2023

SIGNED: _____

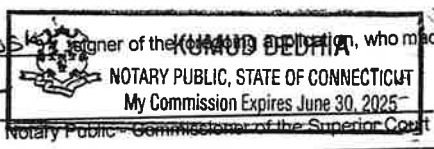
NOTE: Application cannot be scheduled for Public Hearing until 35 days have elapsed from the date of referral to the Stamford Planning Board. If applicant wishes to withdraw application, please notify the Zoning Board at least three (3) days prior to Public Hearing so that the Board may have sufficient time to publicize the withdrawal.

STATE OF CONNECTICUT ss STAMFORD May 27th 20 2023

COUNTY OF FAIRFIELD

Personally appeared Dariusz Lesniewski signer of the zoning application, who made oath to the truth of the contents thereof, before me.

[Handwritten signature]

 NOTARY PUBLIC, STATE OF CONNECTICUT
 My Commission Expires June 30, 2025
 Notary Public - Commissioner of the Superior Court

FOR OFFICE USE ONLY

APPL. #: _____ Received in the office of the Zoning Board: Date: _____

By: _____

Revised 09/02/2020

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MAY 31 2023

ZONING BOARD

Project description

Applicant: Dariusz Lesniewski

Location: 16 Remington St. Stamford CT

1. Existing and Prior Conditions

Built in 1923, 16 Remington St is located in R-MF Multiple Family Residence Design District. It was a 1 ¾ story, multi family home totaling of 2 dwelling units that contained: 4 bedrooms, 2 bathrooms, covered front porch and rear deck with stairs. Single car, detached garage was also located on the back of the property. The lot is 5,244 square foot and is located in the South End neighborhood in Stamford CT and it's listed on the South End Historic District as a contributing building.

Early in 2021, during initial house renovation, existing building was demolished mainly due to discovered damage to structural members and overall poor condition of the structure. Detached garage was also demolished at the same time in order to free necessary square footage to the new proposed structure.

2. Proposed Development

Proposed development includes a partial reconstruction of historic house and construction of two new townhouses attached to be rear of the historic house. Combination of allowable building coverage and required parking spaces, allows only for reconstruction of the first 20 feet of historic building together with the front covered porch.

Reconstructed historic house "Unit A" remains 1 ¾ stories, single family with 2 bedrooms and 2 bathrooms on the upper floor and additional bedroom and bathroom in the basement. The first floor is to have kitchen, living room and half bathroom. Total proposed living area of the historic building will be 1120 SF. During reconstruction, an extra care will be taken to ensure that historic building would come back to its original shape and curb appeal including but not limited to: front porch with Tuscan columns, windows and doors type and placement, roof pitch line with overhangs and dormers, however, the final use of all the architectural details will be decided after consultation with HPAC.

Newly proposed townhouses "Unit B and C"; 4 stories, identical in size, floor layout and each approximately 2300 SF of living floor area will be located at the back of the historic house. Each townhouse to have: one car garage, and mudroom on the lower level, kitchen, living room and half bath on the second floor, 2 bedrooms and 2 full bathrooms on the third floor and 2 more bedrooms and 1 full bathroom on the fourth floor.

Proposed development will be located towards the front of the zoning lot, with parking incorporated into the building via garages and a space at the rear of the building, accessed by a two-way driveway at an existing curb cut on Remington

3. Conformity with the Master Plan

The property is located in Master Plan Category 4 – Residential, Medium Density. The proposed development is consistent with the goals of this category as it would provide residents with pedestrian friendly homes in close proximity to many amenities of the South End area and public transportation.

4. Parking

A total of three (3) parking spaces will be provided on site, two (2) single car garages and one (1) at grade parking space.

5. Other Applications / Procedural Steps

Applicant is seeking Special Permit and Historic Site and Architectural Plan approval pursuant to 7.3B. 2(a) and 19.D.

Special Permit and Site and Architectural Plan and Requested Uses approval from the Zoning Board are required to facilitate the critical reconstruction of a historic building. The project also requires a Coastal Plan Review and the applicant is applying to add the building to Stamford's Cultural Resources Inventory.

The special permits sought are as follows:

- **Building Coverage-** The RM-F zoning district allows a max of 30% building coverage for a maximum of 1,573 sf. Section 7.3C.4.d allows a 25% increase for a maximum of 1,966 sf. The proposed project calls for 1,820 sf.
- **Density.** The permitted density for the site is 2 units under RM-F regulations. The applicant request one additional unit per the 50% density bonus permitted under Section 7.3.C.4.a(2).
- **Rear Setback.** The R-MF zoning requires a min 30' rear setback. Section 7.3.C.4.b allows 50% decrease, requiring a minimum 15' from the rear line. The proposal provides a 23' rear setback.
- **Parking.** Section 12 (Mobility) requires 1.5 parking space per 3+ bedroom unit for a total requirement of 4.5 parking spaces. The application requests a ratio of 1 parking space per unit for a total of 3 spaces per Section 7.3.C.3.
- **Light and Air requirement.** The RM-F district requires a light and air setback of 20ft of which 10ft is require on the Applicant's property. Section 7.3.C.4.e allows for a 50% reduction in the requirement to 5'. The proposal provides 8' Light and Air setback on the east side of the building.

6. Conclusion

The proposed development of 16 Remington St. will preserve the historic character of the neighborhood by bringing demolished building to its prior shape. All details like windows and door types, although upgraded to energy efficiency standards shall remain the same in type and location. Front porch columns and other details shall be consistent with the original building. Exterior finish of the new development will fit into surrounding houses. Hardi cement siding to be used on both, reconstructed historic house and new townhouses. Color samples attached with architectural set A4.0

The applications are consistent with the land use and development goals of the City and, if approved, will provide a positive contribution to South End neighborhood.

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MAY 31 2023

ZONING BOARD

Statement of Findings

Applicant: Dariusz Lesniewski

Location: 16 Remington St. Stamford CT

The following Statement of Findings is being submitted under the Special Permit Application for 16 Remington St. Stamford CT

a. Special permits shall be granted by the reviewing board only upon a finding that the proposed use or structure or the proposed extension or alteration of an existing use or structure is in accord with the public convenience and welfare after taking into account, where appropriate:

- (1) The location and nature of the proposed site including its size and configuration, the proposed size, scale and arrangement of structures, drives and parking areas and the proximity of existing dwellings and other structures.

Response: Proposed building is consistent is size, type and exterior appearance with surrounding building on the same street. Moreover; the size and height of the proposed development meets the requirements of the neighborhoods R-MF zone. Most of the structures on the same street are typically multi-family dwellings. The site is in close proximity to public transportation, trains and the Harbor point area.

- (2) the nature and intensity of the proposed used in relation to its site and the surrounding area. Operations in connection with special permit uses shall not be injurious to the neighborhood, shall be in harmony with the general purpose and intent of these Regulations, and shall not be more objectionable to nearby properties by reason of noise, fumes, vibration, artificial lighting or other disturbances to the health, safety or peaceful enjoyment of the property than the public necessity demands.

Response: The nature and intensity of the proposed structure is consistent with surrounding uses. Newly constructed building will significantly improve neighborhood conditions by replacing old, mostly damage building with new, energy efficient and environmental friendly housing. In addition, proposed building would not change nor increase the nature of the disturbances from its proposed use, since its use would remain unchanged.

- (3) the resulting traffic patterns, the adequacy of existing streets to accommodate the traffic associated with the proposed use, the adequacy of proposed off-street parking and loading, and the extent to which proposed driveways may cause a safety hazard, or traffic nuisance.

Response: The proposed development would not have any significant impact on existing traffic conditions on Remington St. Existing driveway and road access remain unchanged; therefore, no safety hazard or traffic nuisance would increase during and after construction is finished.

Proposed plan provides three (3) off road parking spaces for its occupants. In addition, the neighborhood provides easy access to public transportation.

- (4) The nature of the surrounding area and the extent to which the proposed use or feature might impair its present and future development.

Response: The proposed development would contribute to the use of the site without negatively impacting the surrounding area. The area is capable of absorbing residential development without precluding development of other sites.

- (5) the Master Plan of the City of Stamford and all statements of the purpose and intent of these regulations.

Response: Our project meets the housing goals of the Master Plan by:

- a. preserving the character of residential neighborhood
- b. maintaining affordable housing
- c. encouraging neighborhood revitalization

Additional Finding Pursuant to Section 7.3 of the Zoning Regulations

No special exception shall be granted nor site plan approved pursuant to this Section until the Zoning Board has made a special finding that:

- a) Said use and site plan is compatible with and implements the objectives and policies of Stamford's Master Plan;

Response: The property is located in Master Plan Category 4 – Residential, Medium Density Multifamily. The proposed development is consistent with the goals of this category. It is conveniently located near the train station which can attract young professionals. The close proximity to Harbor Point enhances the quality of life for our residents. It will also provide our residents with pedestrian-friendly homes in close proximity to the many amenities provided by the South End area including restaurants, parks and public access to the South End waterfront.

- b) That said uses and site plan are preferred to a plan conforming to the standard dimensional requirements and use standards of the underlying zone and will not impair the future development of the surrounding area;

Response: As mentioned in Finding (4), proposed development would not negatively impact surrounding area. Proposed building is consistent in size, type and exterior appearance with surrounding building on the same street and it would only increase the attractiveness of the neighborhood.

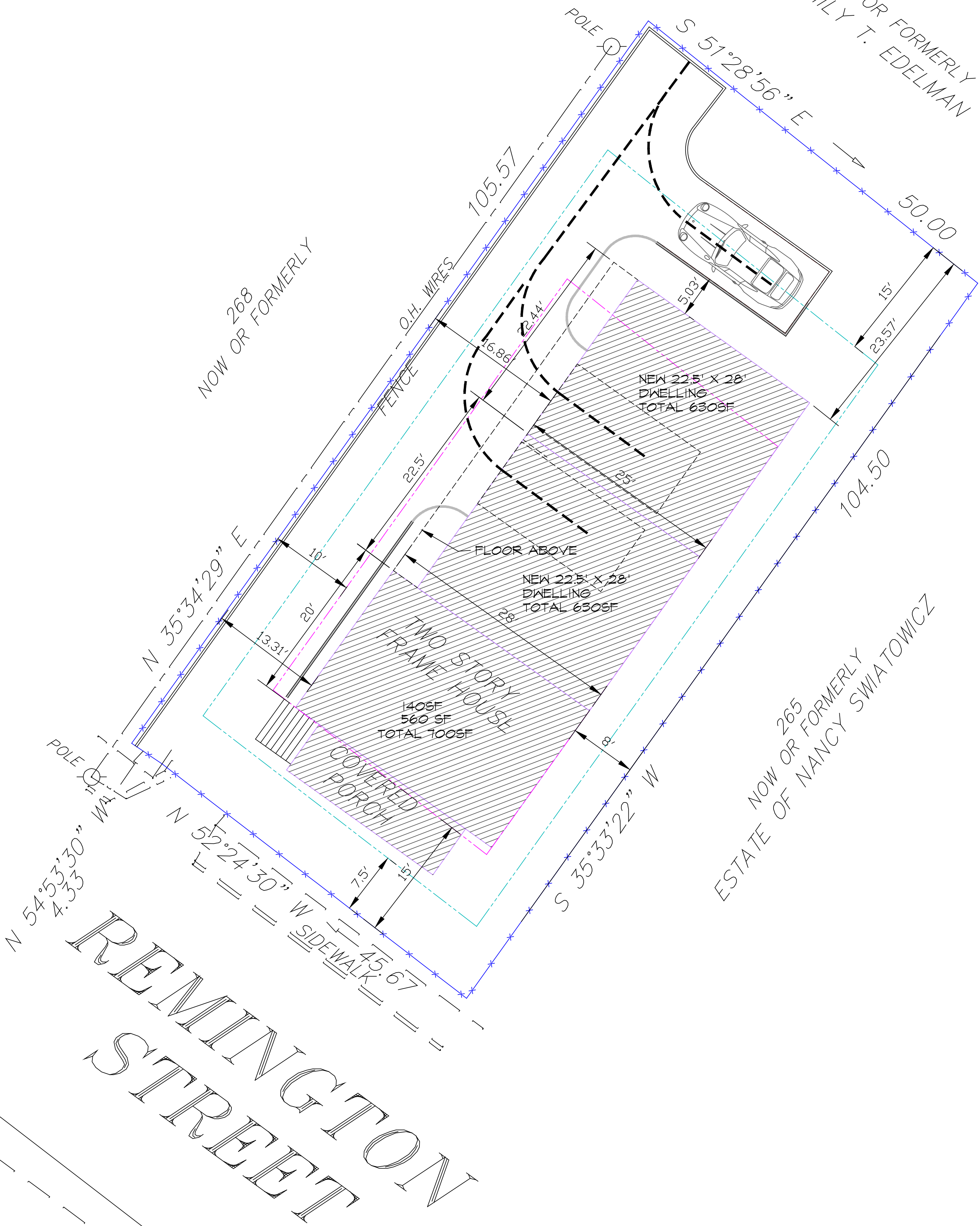
- c) That the proposed use(s) and site plan for development serve to preserve significant historic structure(s) and that the loss of said structure(s) would be detrimental to the neighborhood or district.

Response: The proposed plan includes restoring existing historic structure. The loss of this home would diminish the stock of historic buildings in the South End area.

NOW OR FORMERLY
EMILY T. EDELMAN

NOW OR FORMERLY
268

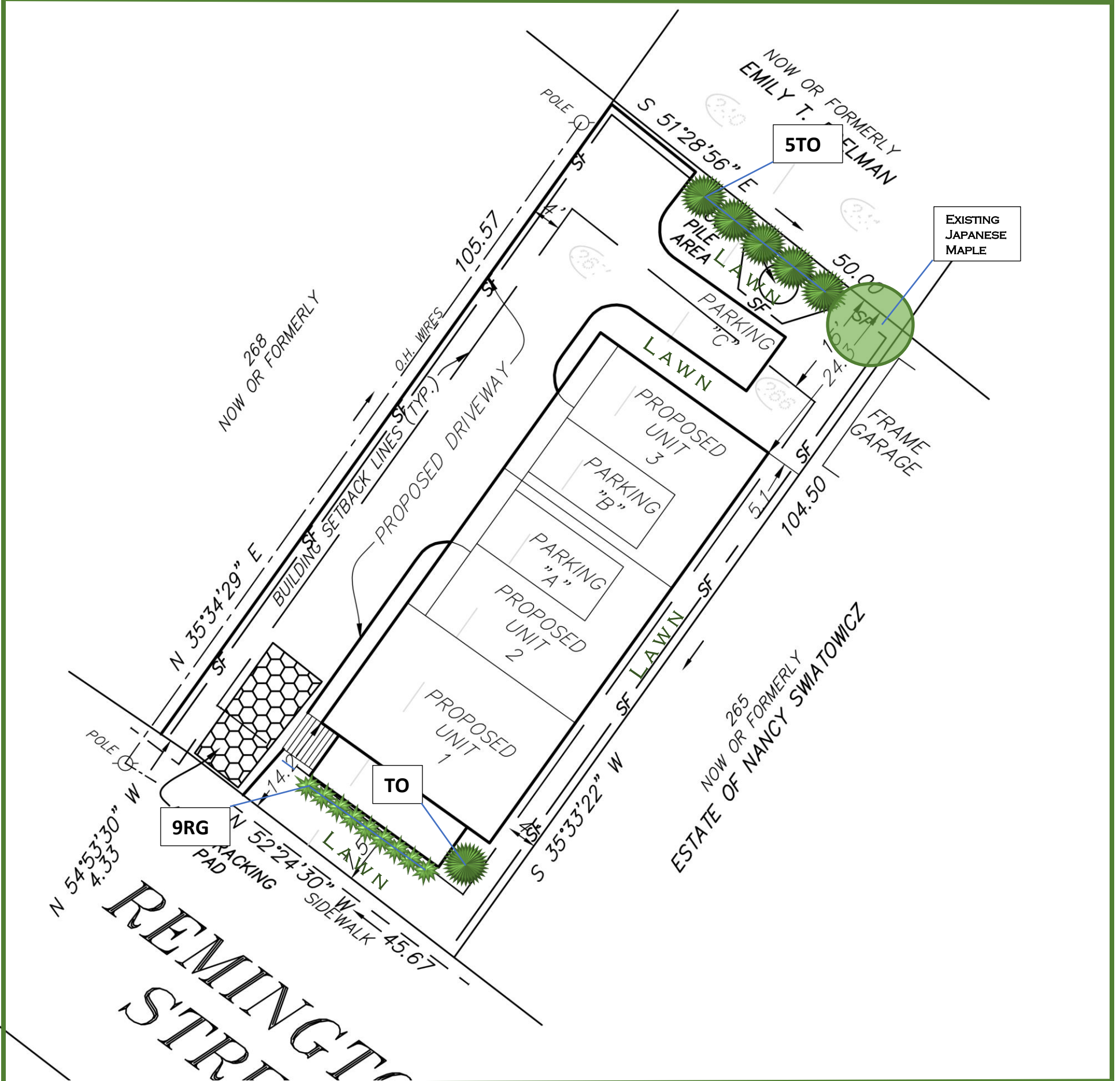
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ESTATE OF NANCY SWIATOWICZ



REMINGTON
STREET

LANDSCAPE PLAN

FOR 16 REMINGTON STREET IN STAMFORD, CT
 BY ALEKSANDRA MOCH, LANDSCAPE DESIGNER, JANUARY 29, 2023



PLANTING SCHEDULE

QTY	KEY	BOTANICAL/COMMON NAME	SIZE	ROOT
SHRUBS:				
6	TO	Thuja occidentalis / North Pole Arborvitae	5'-6'	Cont.
GRASSES:				
9	RG	Karl Foerster Feather Reed Grass	#3	Cont.



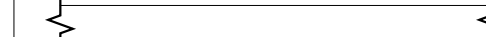
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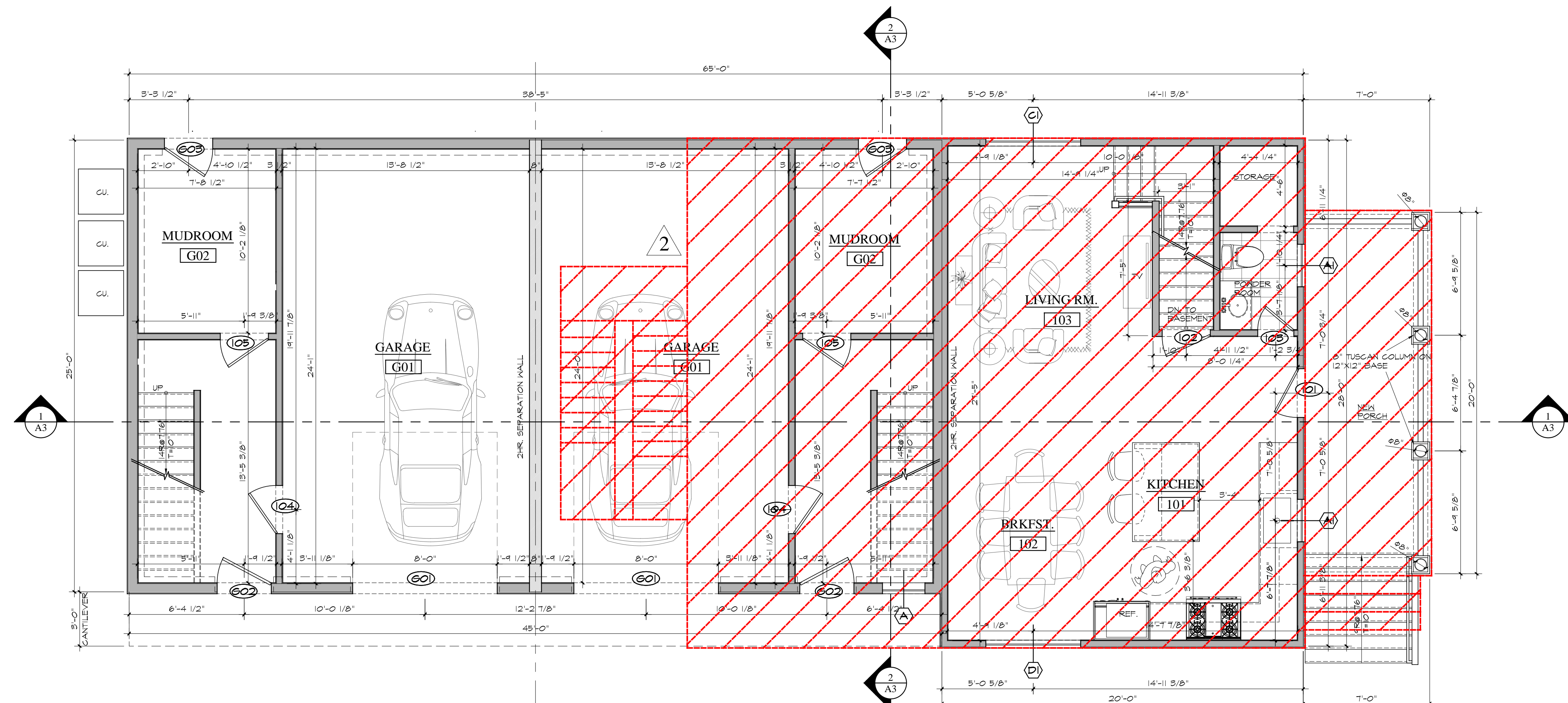
EXISTING CONDITIONS

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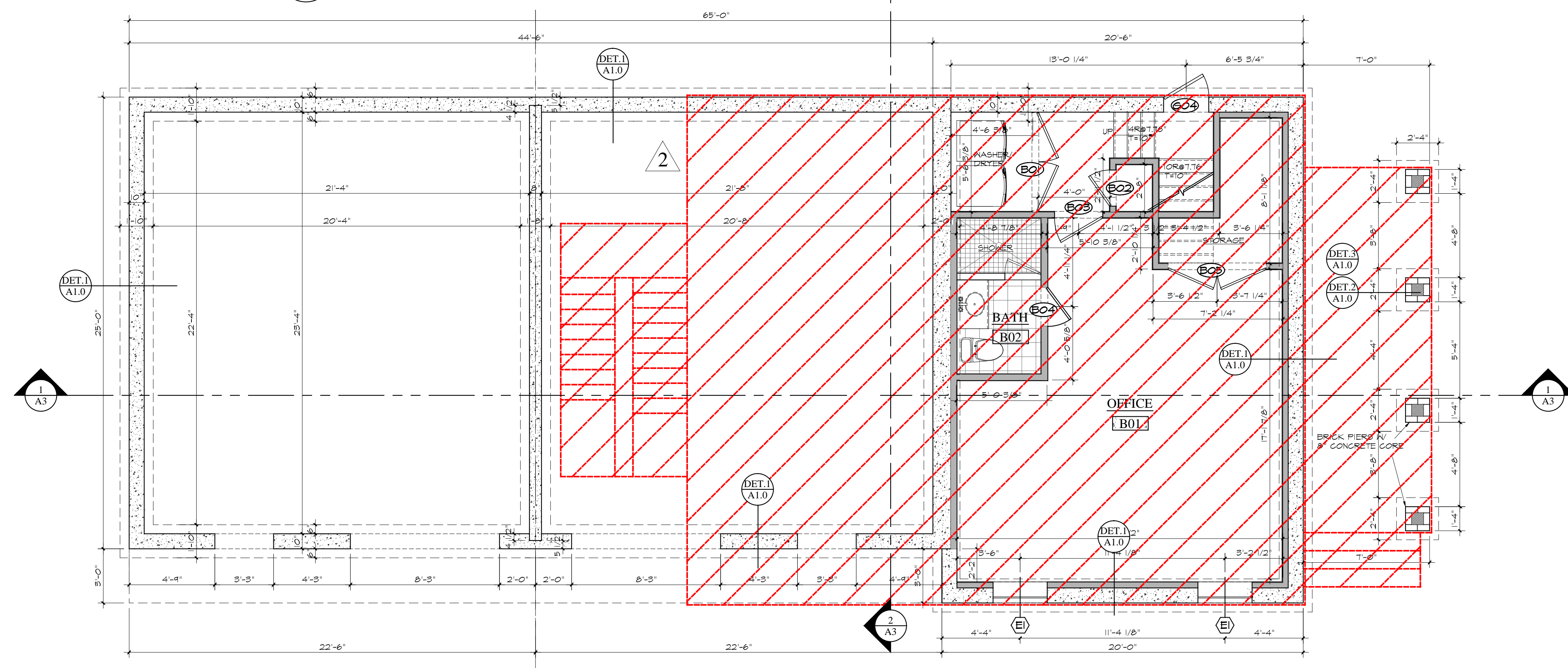
DURING CONSTRUCTION, SHOULD ANY DISCREPANCIES BETWEEN ACTUAL FIELD CONDITIONS AND THESE DRAWINGS BE DISCOVERED, IT IS THE CONTRACTOR'S AND/OR OWNER'S RESPONSIBILITY TO IMMEDIATELY NOTIFY OWNER, DESIGNER OR ARCHITECT.

SYMBOLS LEGEND

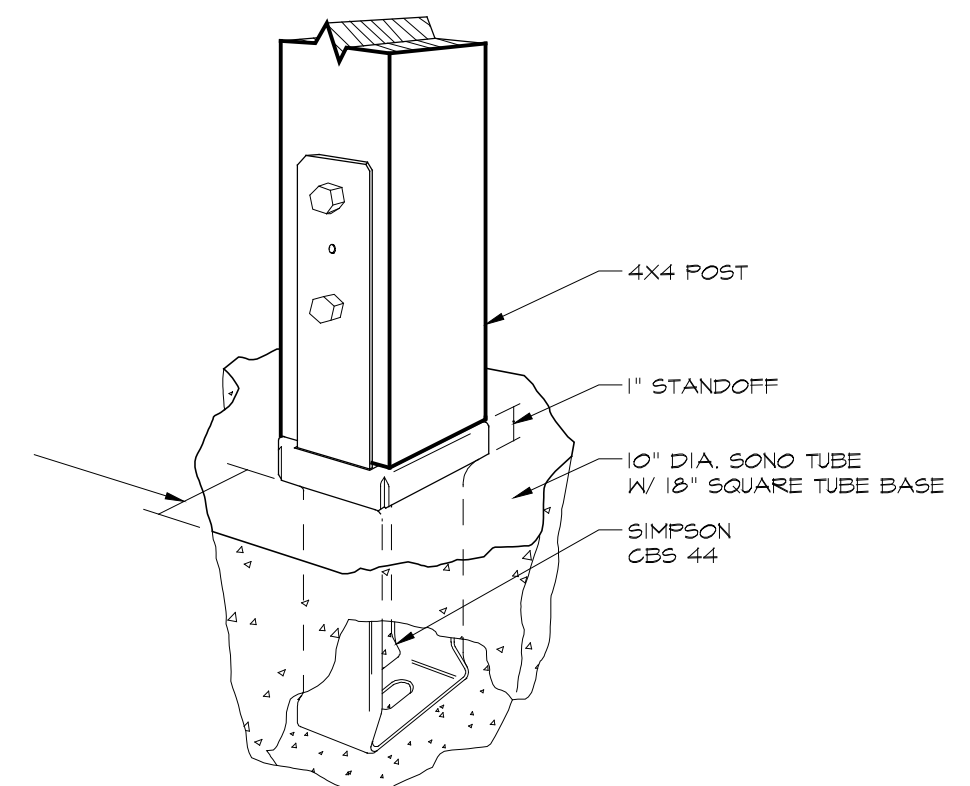
-  NEW PARTITION
-  (EX) TO BE REMOVED
-  (EX) TO REMAIN



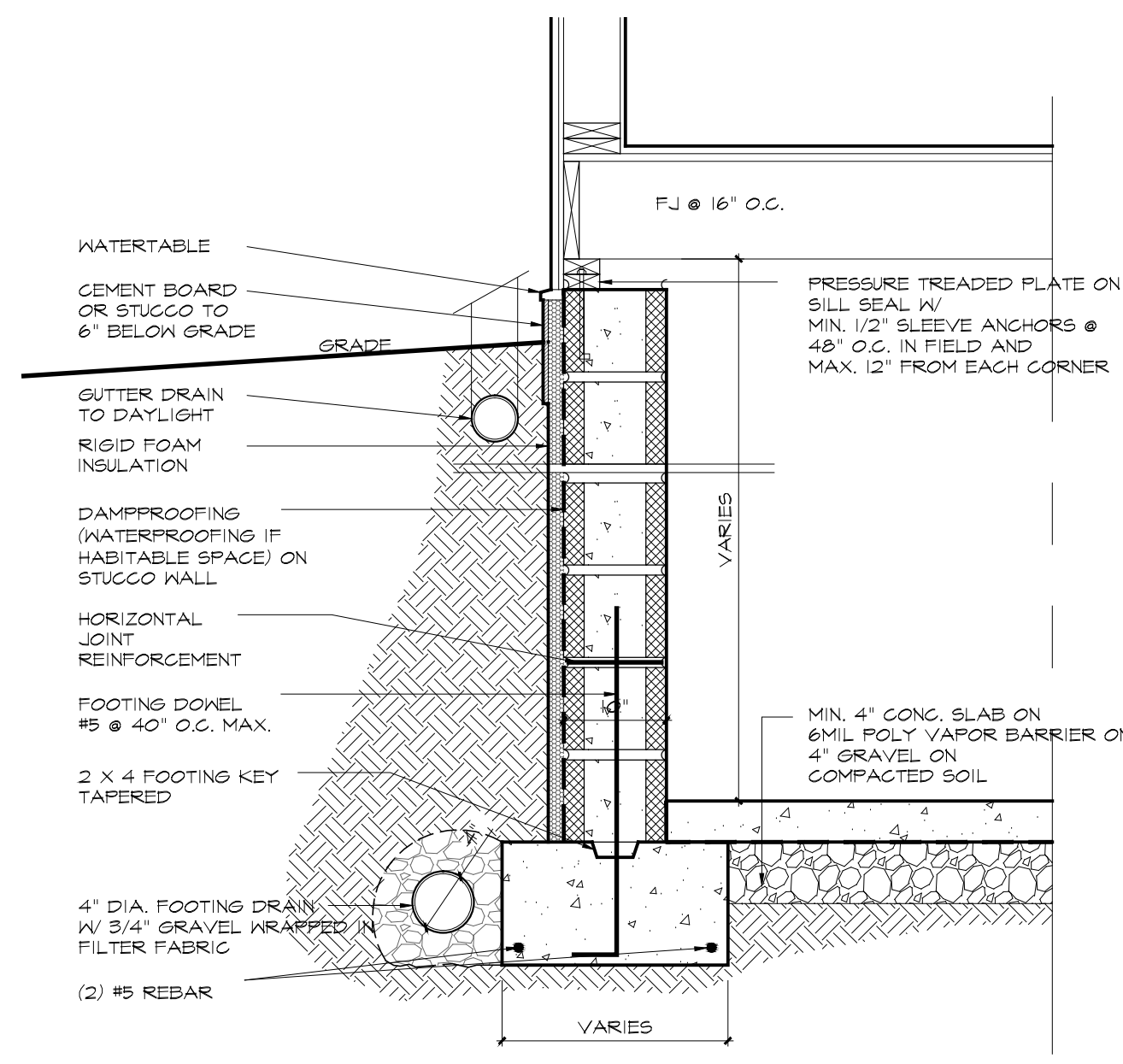
2 LOWER LEVEL / FIRST FLOOR PLAN
 A1.0 SCALE: 1/4"=1'-0"



1 FOUNDATION PLAN
 A1.0 SCALE: 1/4"=1'-0"



DET.2 POST @ SONOTUBE
 A1 NO SCALE



DET.1 FOUNDATION WALL TYPICAL
 A1 SCALE: 3/4"=1'-0"

Revised:	REVISIONS PER HPAC MEETING	Date:	3/14/2023
	REVISION # 2	Date:	5/22/2023

OWNERSHIP AND USE OF DOCUMENTS: Drawings and specifications, as instruments of professional service, are and shall remain the property of the Architect. These documents are not to be used, in whole or in part, for any other projects or purposes, or by other parties, than those properly authorized by contract or without the written authorization.

Dwg. Name:
FLOOR PLANS



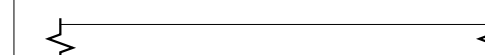
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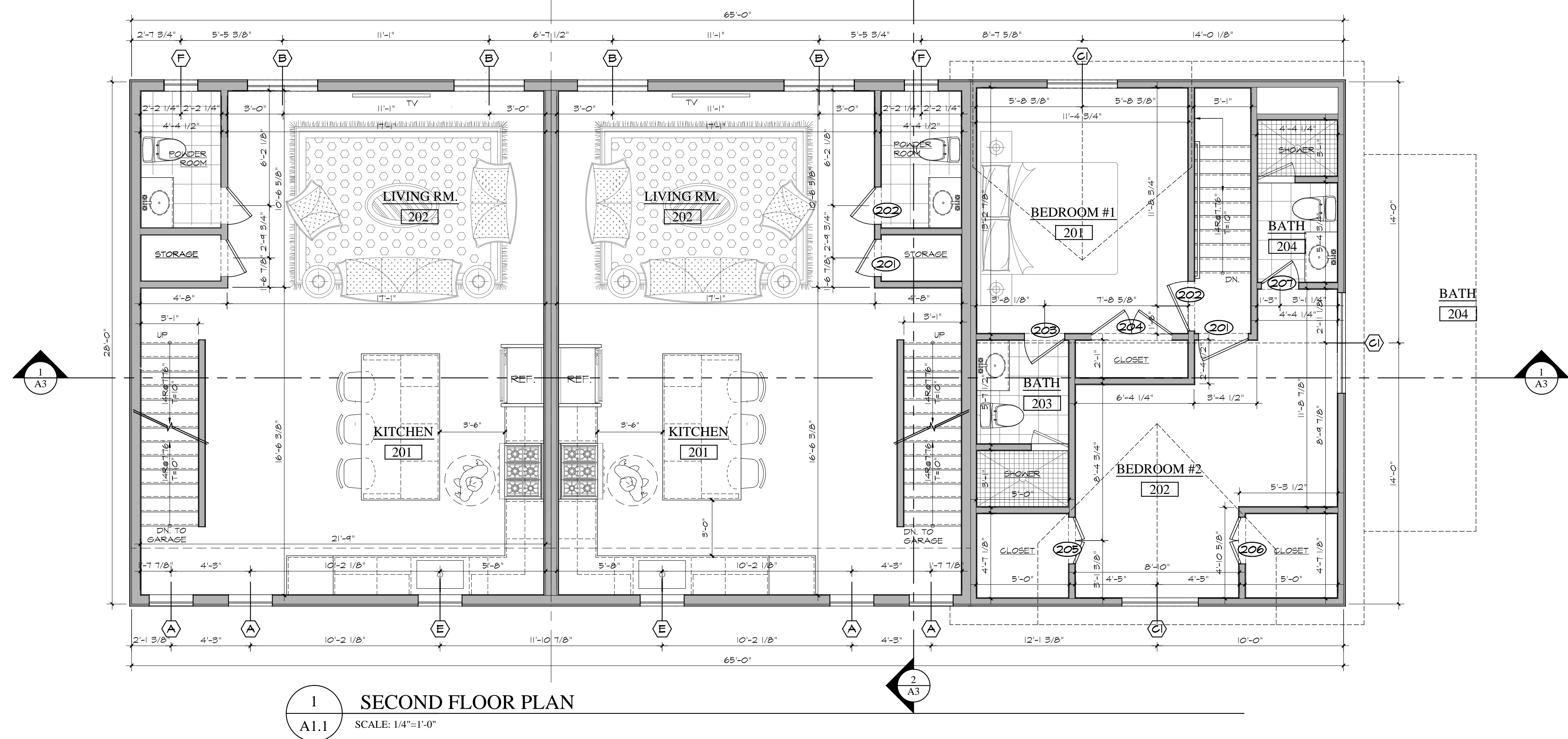
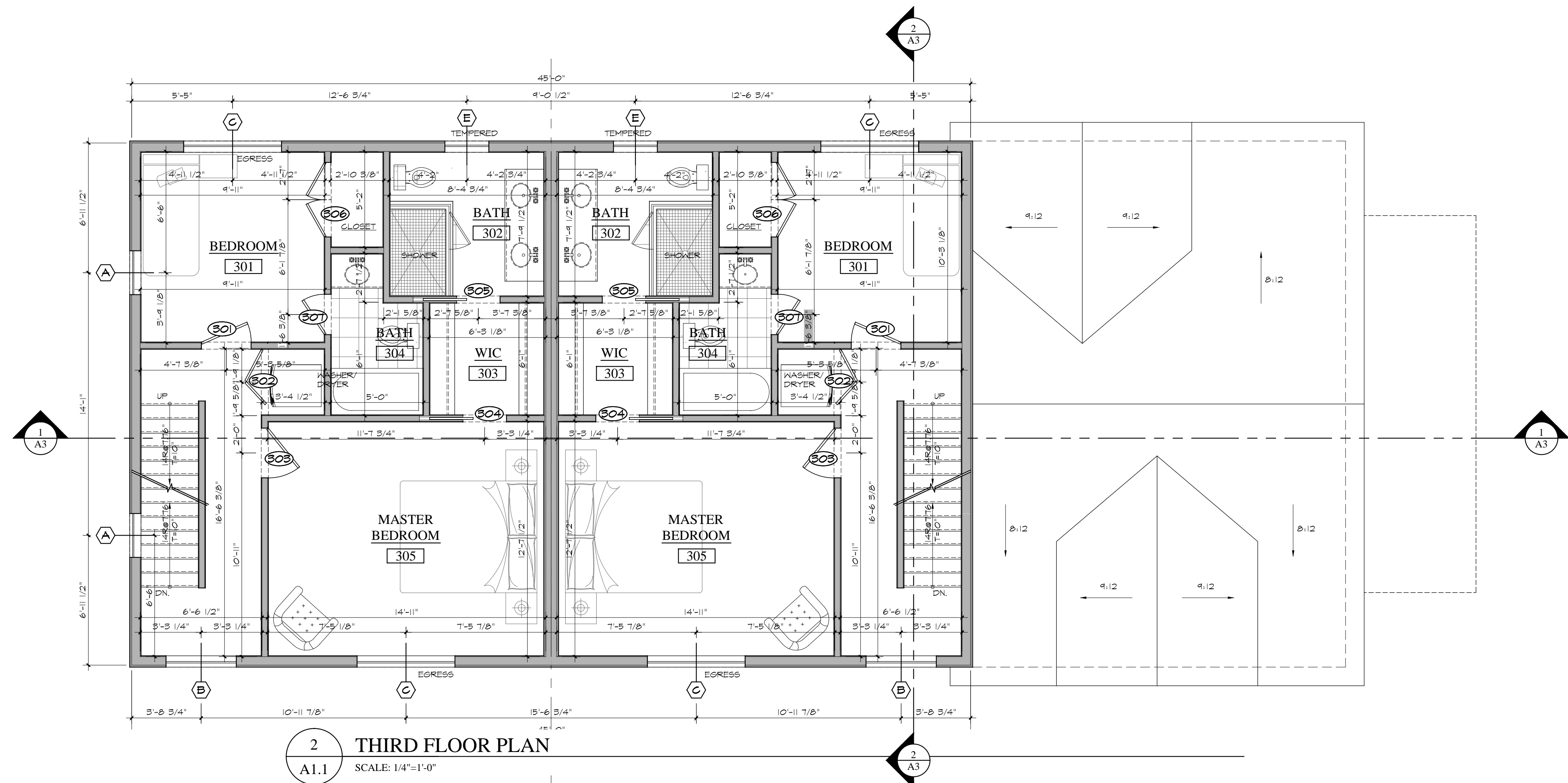
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SYMBOLS LEGEND

-  NEW PARTITION
-  (EX) TO BE REMOVED
-  (EX) TO REMAIN



Revision:	Date:
REVISIONS PER HPAC MEETING	3/14/2023

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Dwg. Name:
FLOOR PLANS




General Notes:

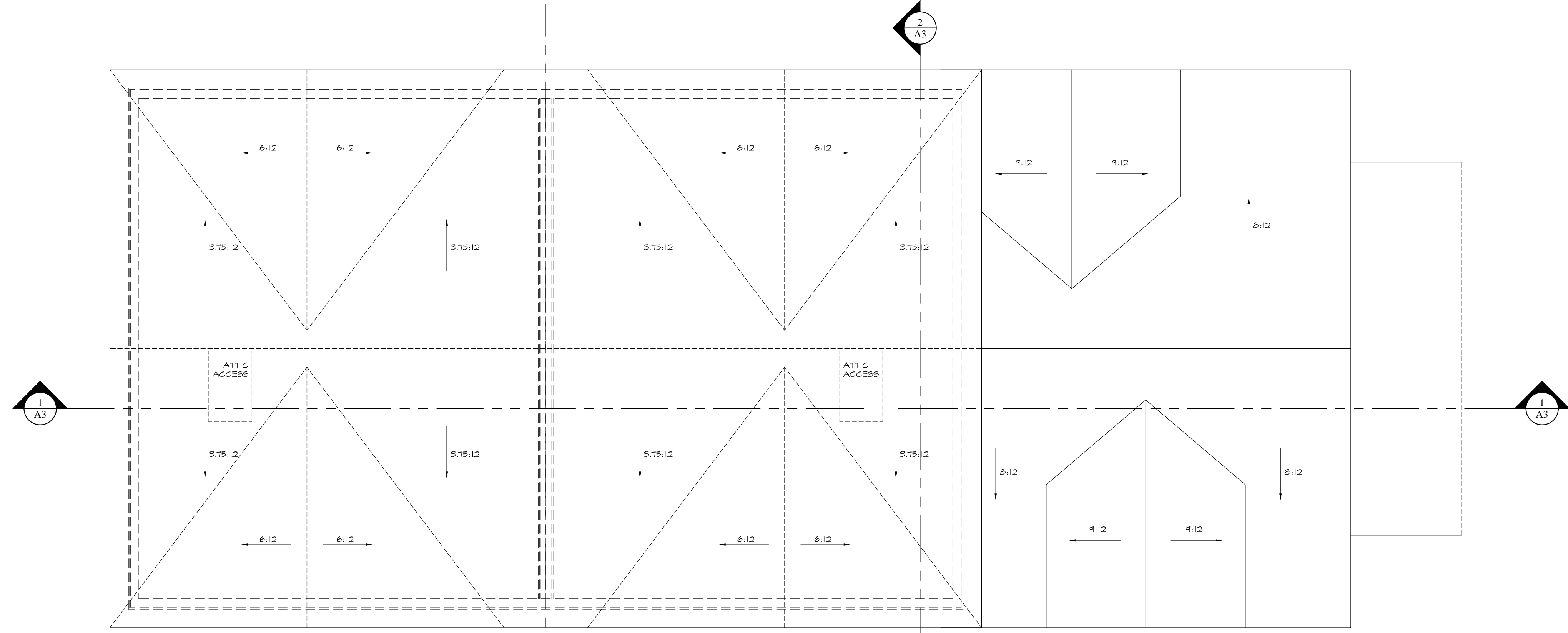
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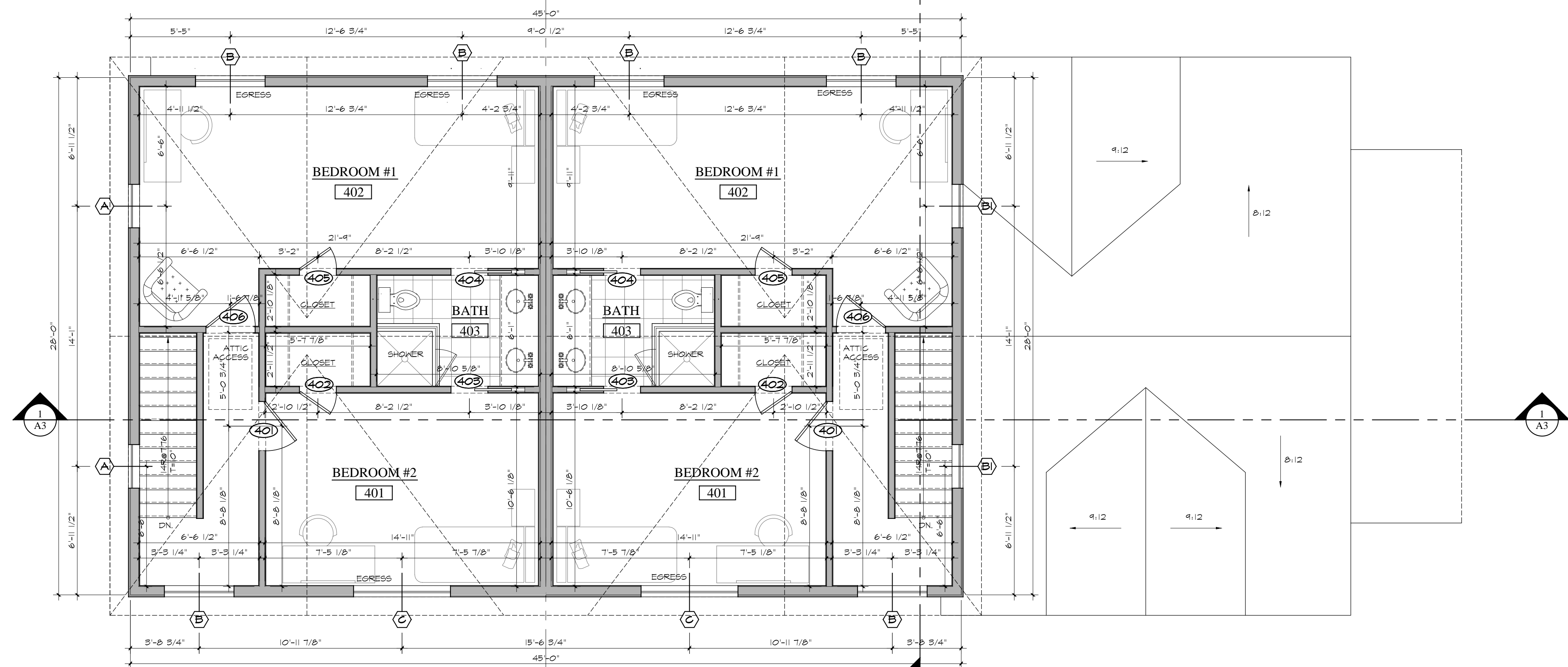
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SYMBOLS LEGEND

-  NEW PARTITION
-  (EX) TO BE REMOVED
-  (EX) TO REMAIN



2 ROOF / ATTIC PLAN
 A1.2 SCALE: 1/4"=1'-0"



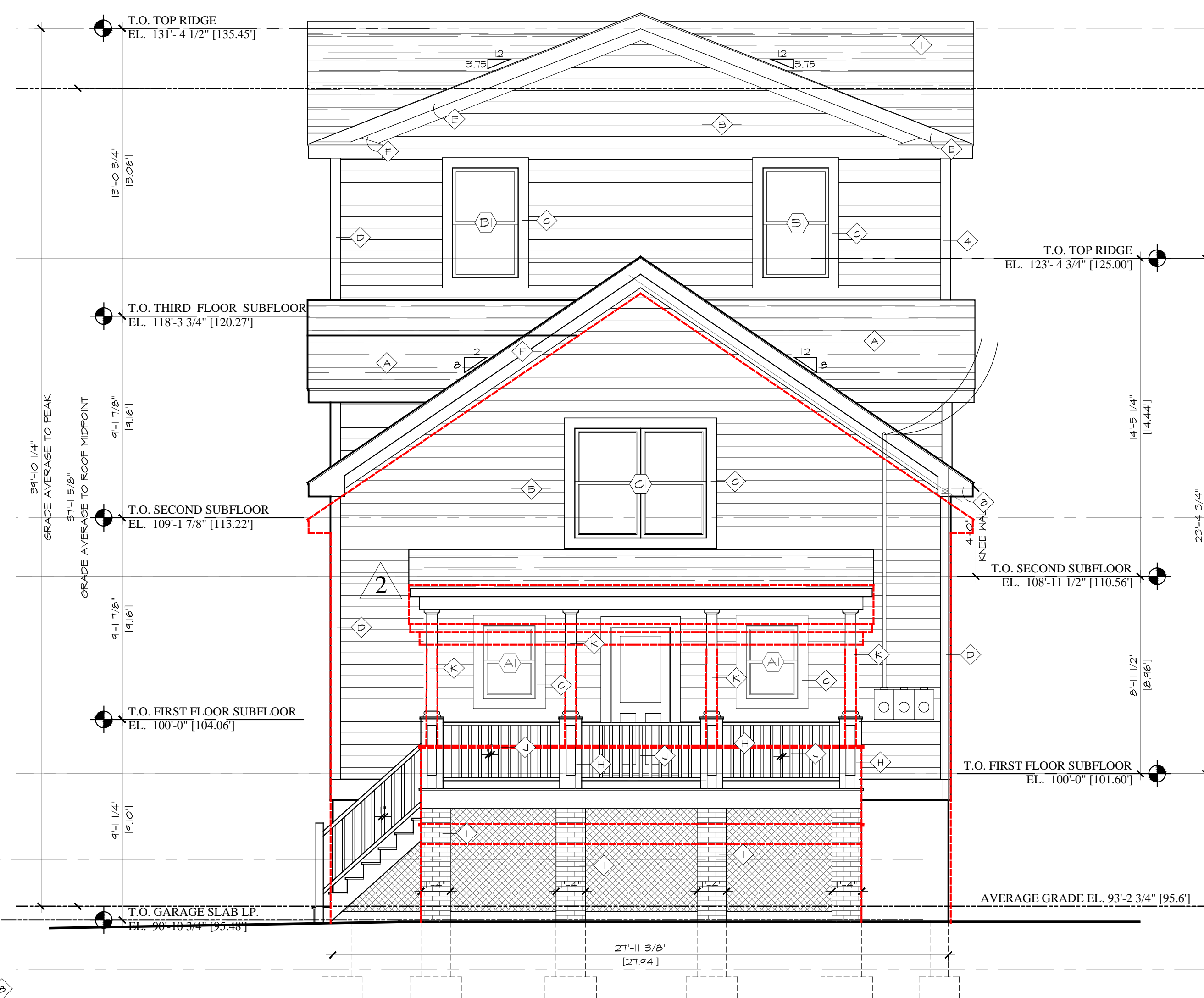
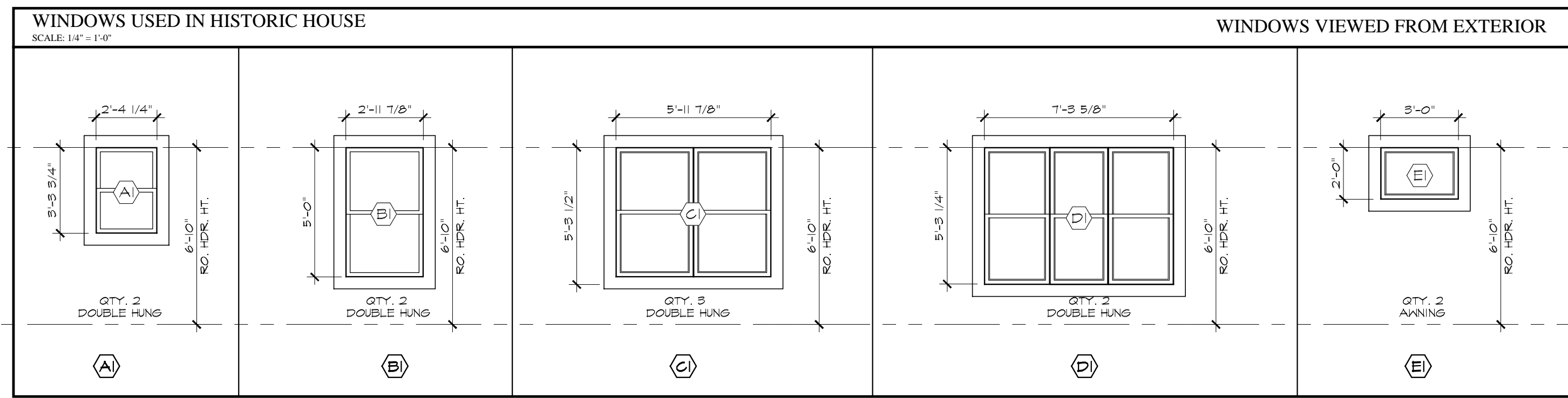
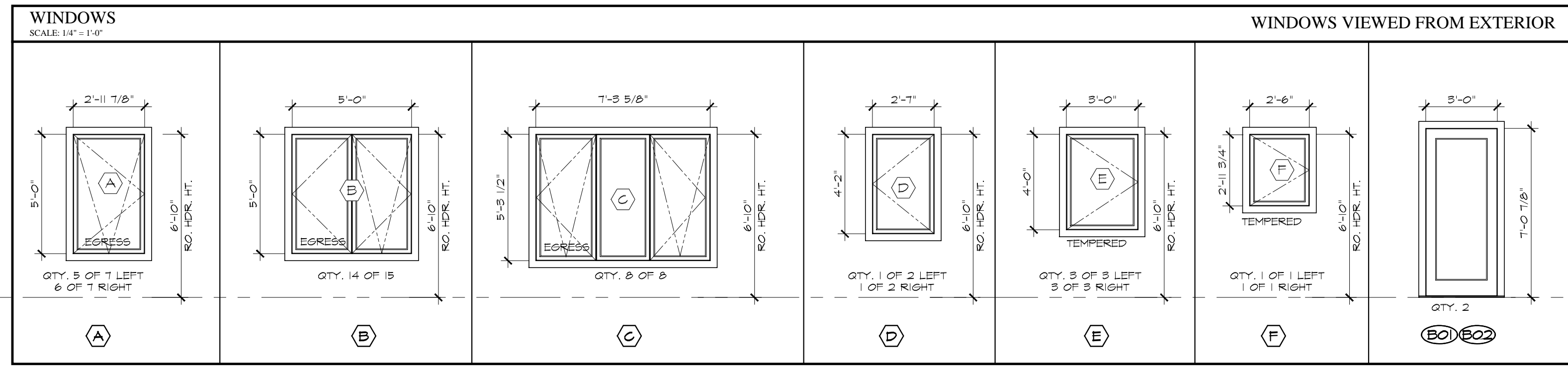
1 FORTH FLOOR PLAN
 A1.2 SCALE: 1/4"=1'-0"

Revision:	Date:
REVISIONS PER HPAC MEETING	3/14/2023

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Dwg. Name:
FLOOR PLANS

Date:	1/25/2023	A1.2
Scale:	AS NOTED	



- General Notes:
- ELEVATION / SECTION NOTES**
 (NOTES SHALL BE CONSIDERED TYPICAL FOR ITEMS IDENTIFIED AND SHALL APPLY AT ALL SAME AND SIMILAR CONDITIONS. ALL LOCATIONS MAY NOT BE NOTES.)
- 30 YEAR ARCH. ASPHALT SHINGLE
 - CEMENT SIDING, HARDIE ARCTIC WHITE
 - 5/4" X 4 WINDOW CASING
 - 5/4 X 6 CORNER BOARD
 - 1-1/4" AZEK SILL TAPERED
 - 1 X 8 FASCIA BOARD
 - VERTICAL SIDING TBD.Q
 - 1/2" SOFFIT W/ COR-A-VENT
- HISTORIC BUILDING NOTES**
 (NOTES SHALL BE CONSIDERED TYPICAL FOR ITEMS IDENTIFIED AND SHALL APPLY AT ALL SAME AND SIMILAR CONDITIONS. ALL LOCATIONS MAY NOT BE NOTES.)
- 30 YEAR ARCH. ASPHALT SHINGLE
 - CEMENT SIDING, HARDIE ARCTIC WHITE
 - 5/4" X 6 WINDOW CASING, WHITE
 - 5/4 X 6 CORNER BOARD, WHITE
 - 1 X 8 FASCIA BOARD
 - 5/4" X 6 FREEZE BOARD
 - 1/2" SOFFIT W/ COR-A-VENT
 - 12X12 POST BASE, PAINTED
 - BRICK PIER
 - CEDAR RAILING, 1" BALUSTERS PAINTED
 - 8" TUSCAN COLUMN, PAINTED
 - MAILBOXES

Revision:	Date:
REVISIONS PER HPAC MEETING	3/14/2023
REVISION #2 2	5/22/2023

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Dwg. Name:
ELEVATIONS WINDOWS SCHEDULE

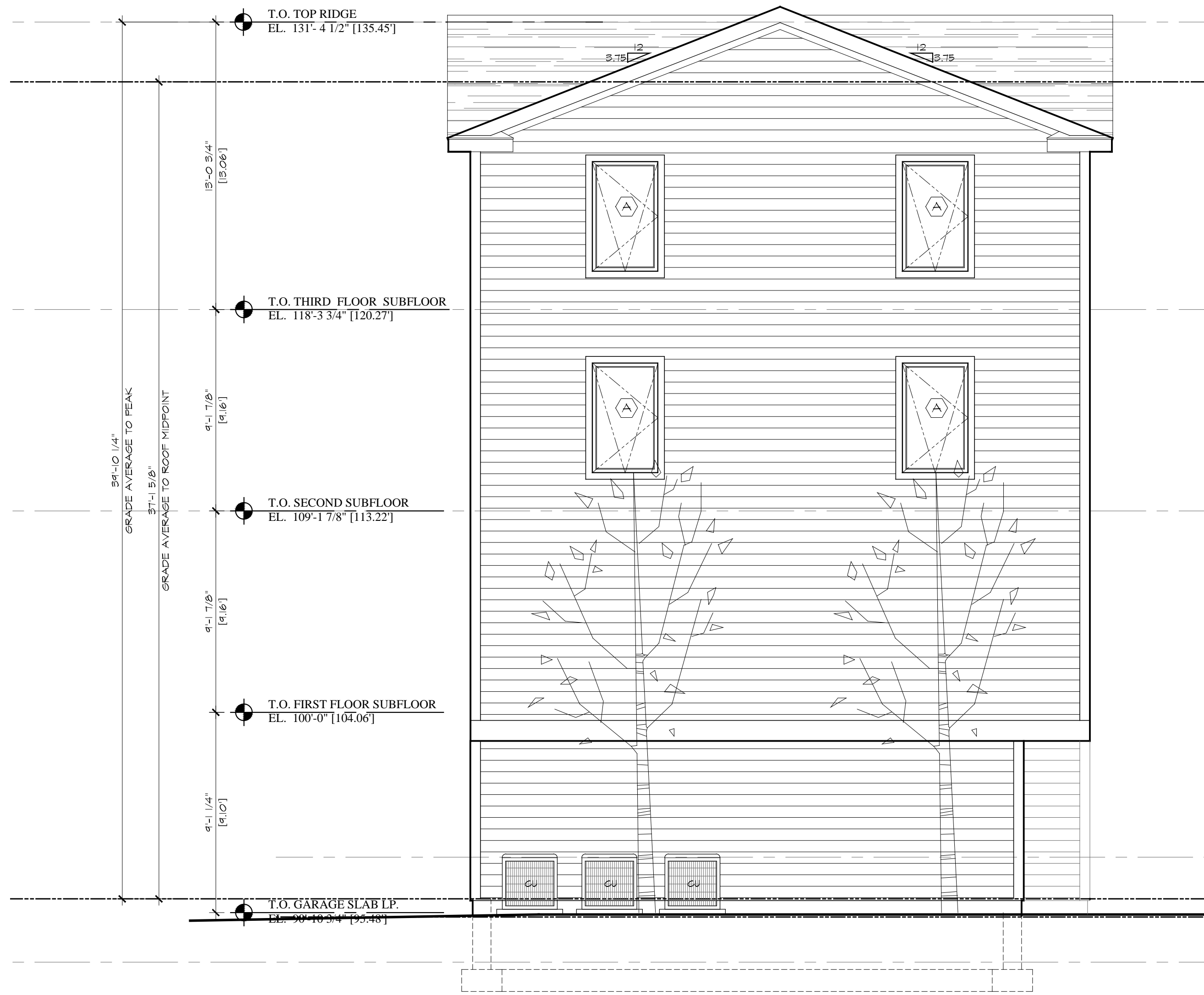
Date:	1/25/2023	A2.0
Scale:	as shown	

General Notes:
 ELEVATION / SECTION NOTES
 (NOTES SHALL BE CONSIDERED TYPICAL FOR ITEMS IDENTIFIED AND SHALL APPLY AT ALL SAME AND SIMILAR CONDITIONS. ALL LOCATIONS MAY NOT BE NOTES.)

1. 30 YEAR ARCH. ASPHALT SHINGLE
2. CEMENT SIDING, HARDIE ARCTIC WHITE
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HISTORIC BUILDING NOTES
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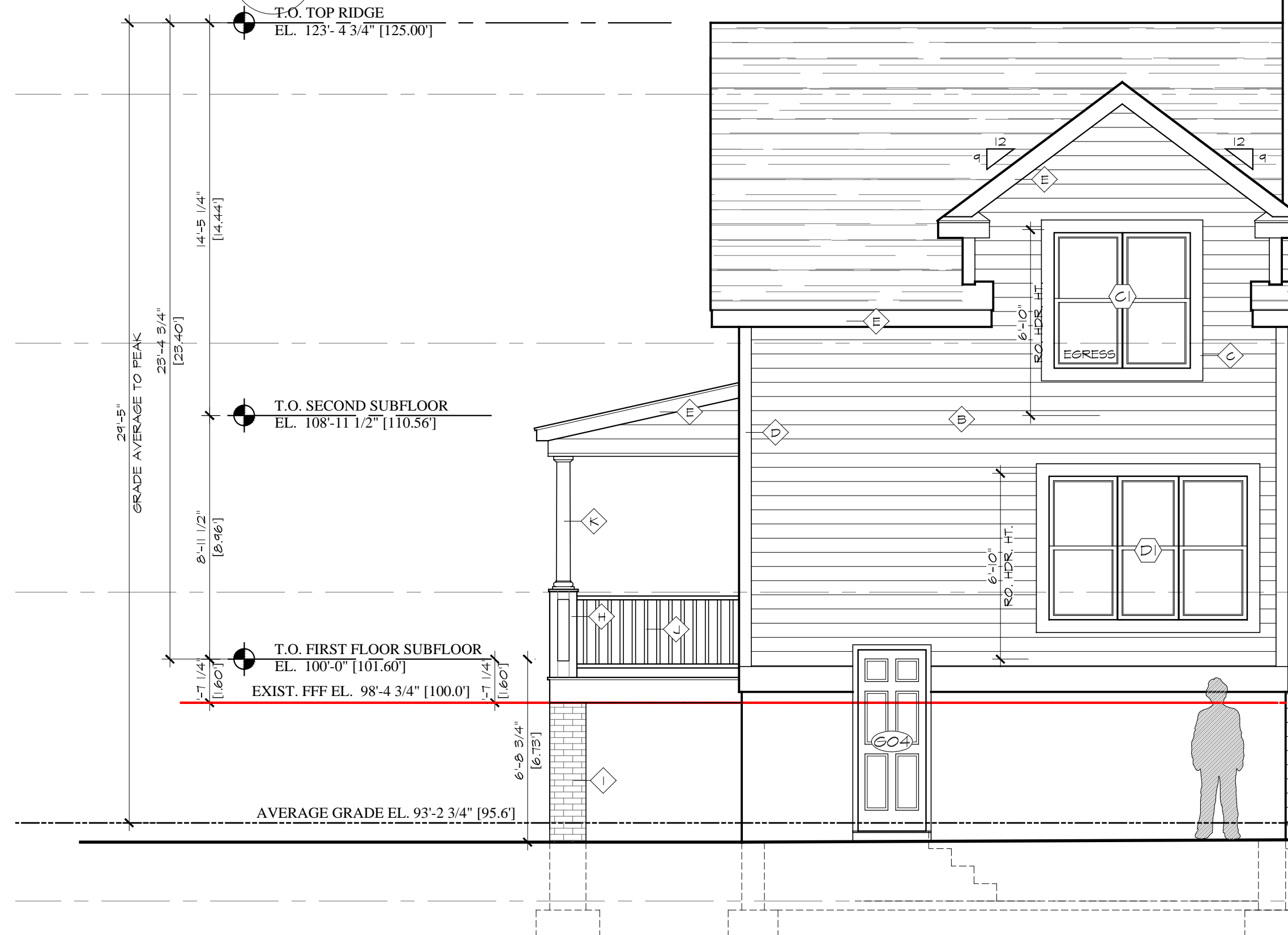
- A. 30 YEAR ARCH. ASPHALT SHINGLE
- B. CEMENT SIDING, HARDIE ARCTIC WHITE
- C. 5/4" X 6 WINDOW CASING, WHITE
- D. 5/4 X 6 CORNER BOARD, WHITE
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- F. 5/4" X 6 FREEZE BOARD
- G. 1/2" SOFFIT W/ COR-A-VENT
- H. 12X12 POST BASE, PAINTED
- I. BRICK PIER
- J. CEDAR RAILING, 1" BALUSTERS PAINTED
- K. 8" TUSCAN COLUMN, PAINTED
- L. MAILBOXES



2 REAR ELEVATION

A2.1 SCALE: 1/4"=1'-0"

T.O. TOP RIDGE
 EL. 123'-4 3/4" [125.00]



1 RIGHT ELEVATION

A2.1 SCALE: 1/4"=1'-0"



Revision: REVISIONS PER HPAC MEETING Date: 3/14/2023

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Dwg. Name: ELEVATIONS

Date: 1/25/2023
 Scale: as shown
 Sheet: A2.1

General Notes:

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ROOF SHEATHING
 UNLESS OTHERWISE NOTED, ALL ROOF SHEATHING SHALL BE 15/32" (32/16) CDX PLYWOOD, NAILED WITH 10d COMMON NAILS AT 6" O.C. BOUNDARY, 6" O.C. ALL OTHER EDGES AND 12" O.C. FIELD.

FLOOR SHEATHING
 UNLESS OTHERWISE NOTED, ALL FLOOR SHEATHING SHALL BE 23/32" (45/24) T&G UNDERLAYMENT OR OSB, ATTACHED TO JOISTS WITH #10 SCREWS AT 8" & 10" O.C. ALL FLOOR ADHESIVE SHALL BE MANUFACTURED BY "TOPS-PRO" OR "JAWS", NO SUBSTITUTE. INSTALL FLOOR SHEATHING SCREWS NO MORE THAN ONE HOUR AFTER APPLICATION OF THE ADHESIVE. UPON COMPLETION OF FLOOR FRAMING, INSTALL SCREWS THROUGH THE "DIAMOND HOLE" ON THE BOTTOM OF EACH IT JOIST HANGER. INSTALL 6 SCREWS AT 6" ON CENTER THROUGH THE BOTTOM OF THE SHEATHING INTO THE BOTTOM PLATE AT EACH END OF ALL NONBEARING WALLS.

TJI INSTALLATION AND NAILING REQUIREMENTS

WARNING
 Joists are unstable until braced laterally.

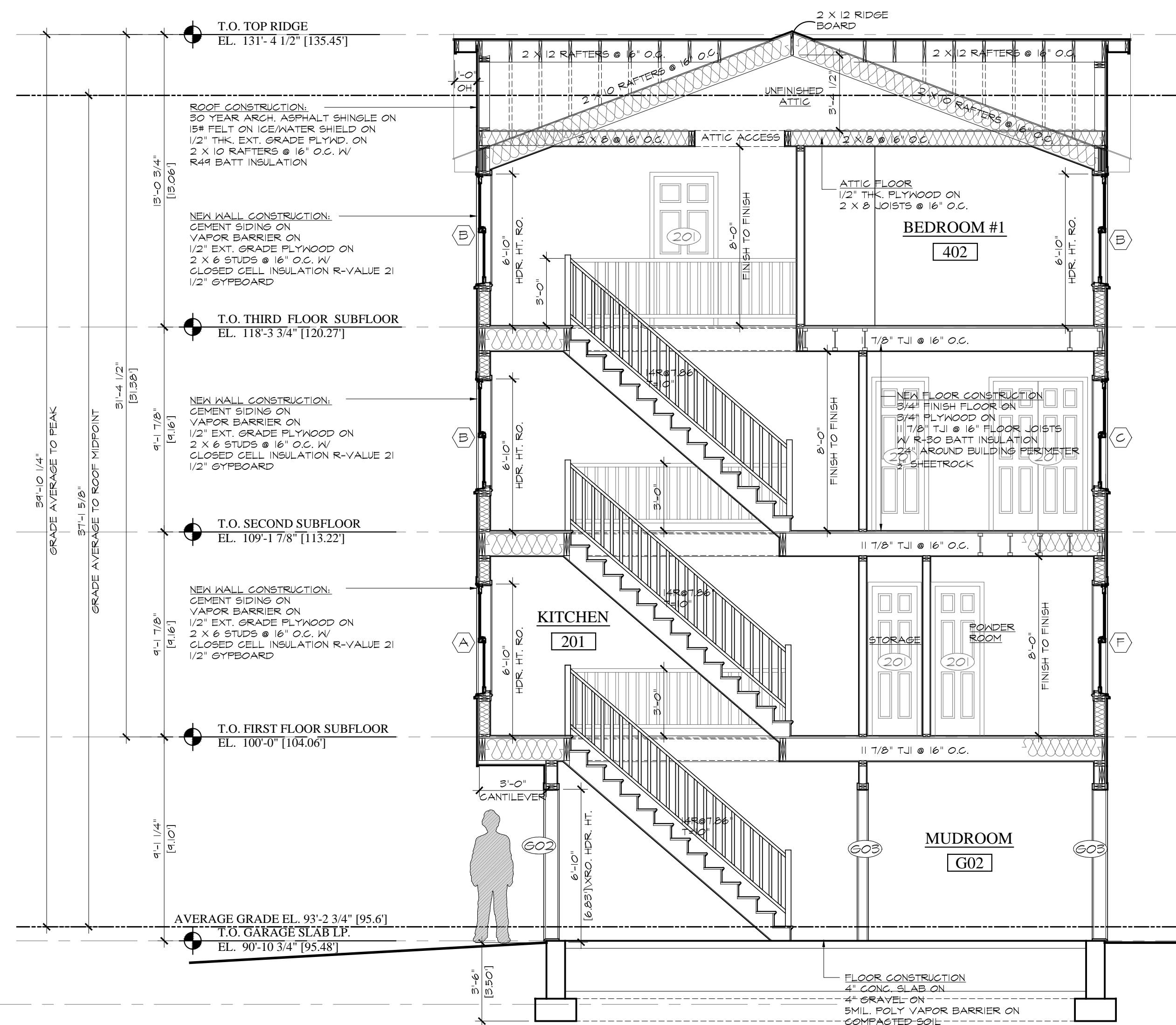
Bracing Includes:
 • Blocking • Shoring
 • Hangers • Rim Board
 • Stud Lines • Rein Joist

DO NOT allow workers to walk on joists until braced. DO NOT stack building materials on unbraced joists.

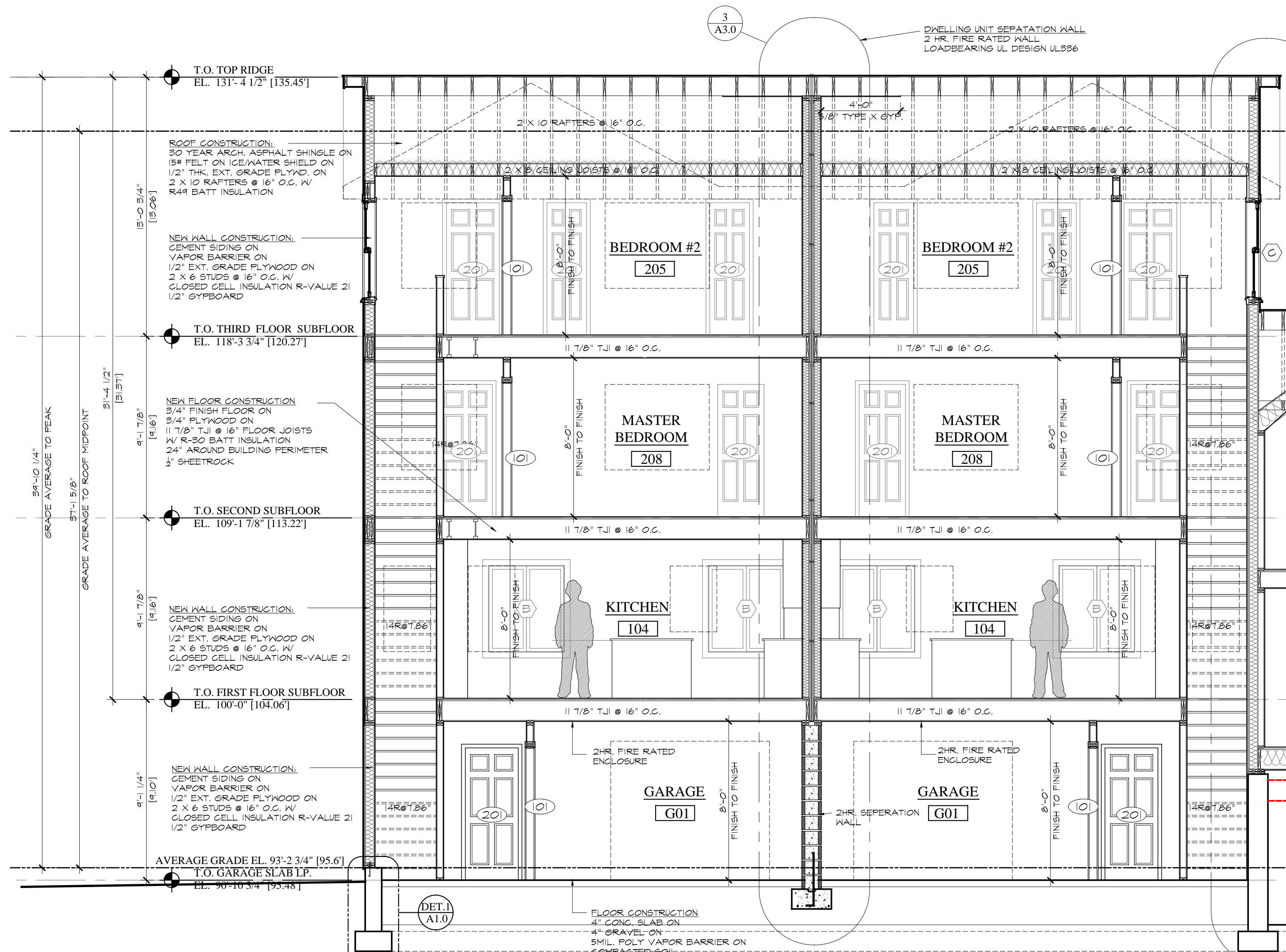
WARNING NOTES:
 Lack of proper bracing during construction can result in serious accidents. Under normal conditions if the following guidelines are observed, accidents will be avoided.
 1. Install all blocking, hangers, rim boards and rim joists at TJI joist end supports.
 2. Establish a permanent deck (sheathing), nailed to the first 4 feet of joists at the end of the bay or braced end wall.
 3. Safety strut lines of 1/4" (minimum) must be nailed to a braced end wall or sheathed area.
 4. Sheathing must be properly nailed to each TJI joist before additional loads can be placed on the system.
 5. Ends of cantilevers require stud lines on both the top and bottom flanges.
 6. TJI joist flanges must remain straight within a tolerance of 1/8" from true alignment.

TJI NAILING REQUIREMENTS

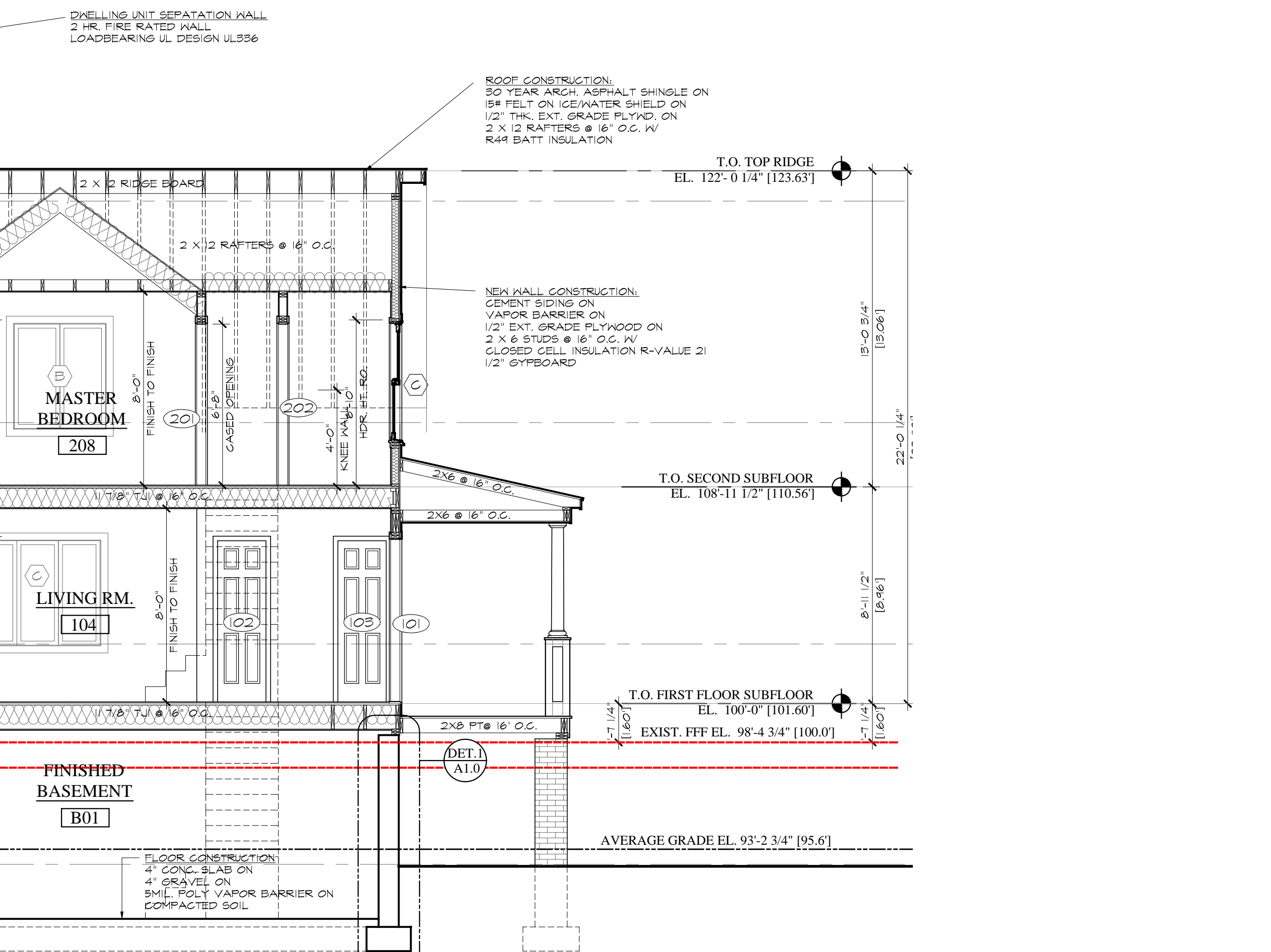
TJI joists at bearings: Two 8d (2 1/2") box nails (1 each side), 1 1/2" minimum from end.
 Blocking panels, rim joist or rim board to bearing plate:
 TJI blocking panels or rim joist: 10d (3") box nails at 6" o.c.
 Trus Joist rim board: Toenail with 10d (3") box nails at 6" o.c. or 16d (3 1/2") box nails at 12" o.c.
 Shear transfer: Connections equivalent to decking nail schedule.
 Rim board, rim joist or closure to TJI joist:
 1 1/2" width or less: Two 10d (3") box nails, one each at top and bottom flange.
 TJI Pro™ 130, 350, 100C & 300C rim joist: Two 16d (3 1/2") box nails, one each at top and bottom flange.
 TJI Pro™ 550 & 400C rim joist: Toenail joist to rim joist with one 10d (3") box nail each side of joist top flange.
 2x4 minimum squash blocks: Two 10d (3") box nails, one each at top and bottom flange.



2 BUILDING CROSS SECTION
 SCALE: 1/4"=1'-0"



1 BUILDING SECTION
 SCALE: 1/4"=1'-0"



2 BUILDING SECTION
 SCALE: 1/4"=1'-0"

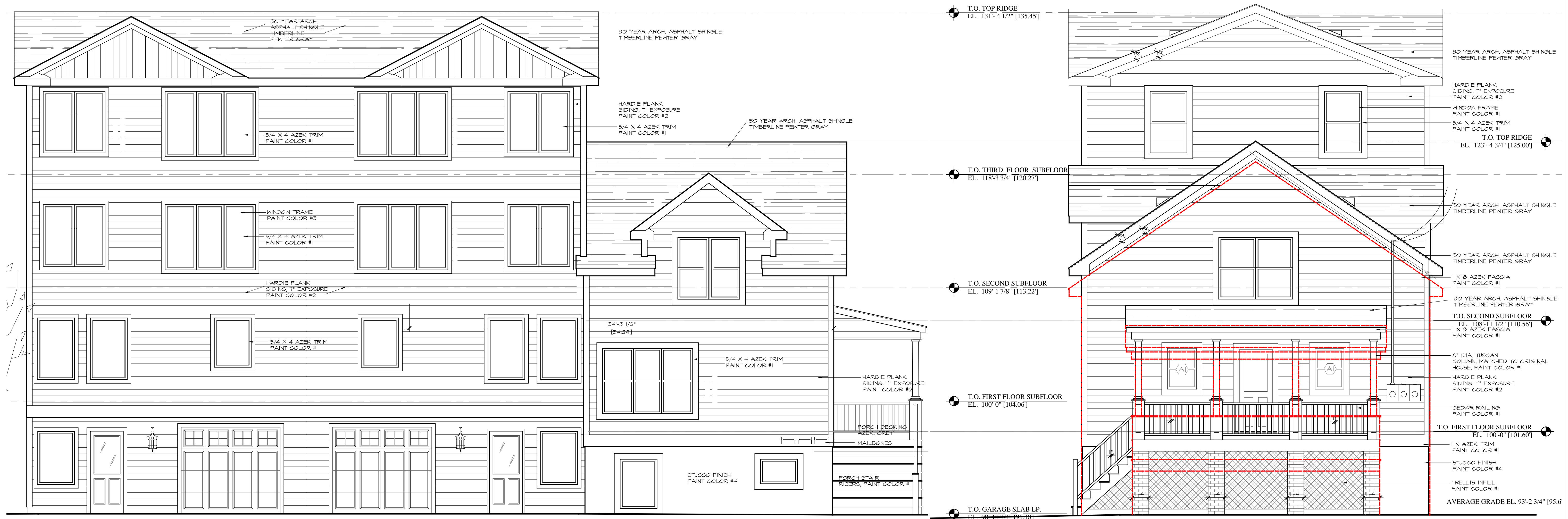
Revision:	Date:
REVISIONS PER HPAC MEETING	3/14/2023

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Draw. Name:
BUILDING SECTION

PERMIT SET

- PAINT COLOR #1
USED AT HISTORIC HOUSE AND NEW TOWNHOUSES
EXTERIOR PAINT, LOW LUSTER
DECORATORS WHITE CC-144
BY BENJAMIN MOORE
- PAINT COLOR #2
USED AT HISTORIC HOUSE AND NEW TOWNHOUSES
HARDIE PLANK, SMOOTH FINISH
FACTORY PAINTED 'ARCTIC WHITE'
- PAINT COLOR #3
USED AT HISTORIC HOUSE AND NEW TOWNHOUSES
EXTERIOR WINDOW FRAME
FACTORY APPLIED
'ANTHRACITE GRAY'
- PAINT COLOR #4
USED AT HISTORIC HOUSE AND NEW TOWNHOUSES
EXTERIOR PAINT, LOW LUSTER
GOVENTRY GRAY HC-164
BY BENJAMIN MOORE



Revised:	Date:
REVISIONS PER HPAC MEETING	3/14/2023

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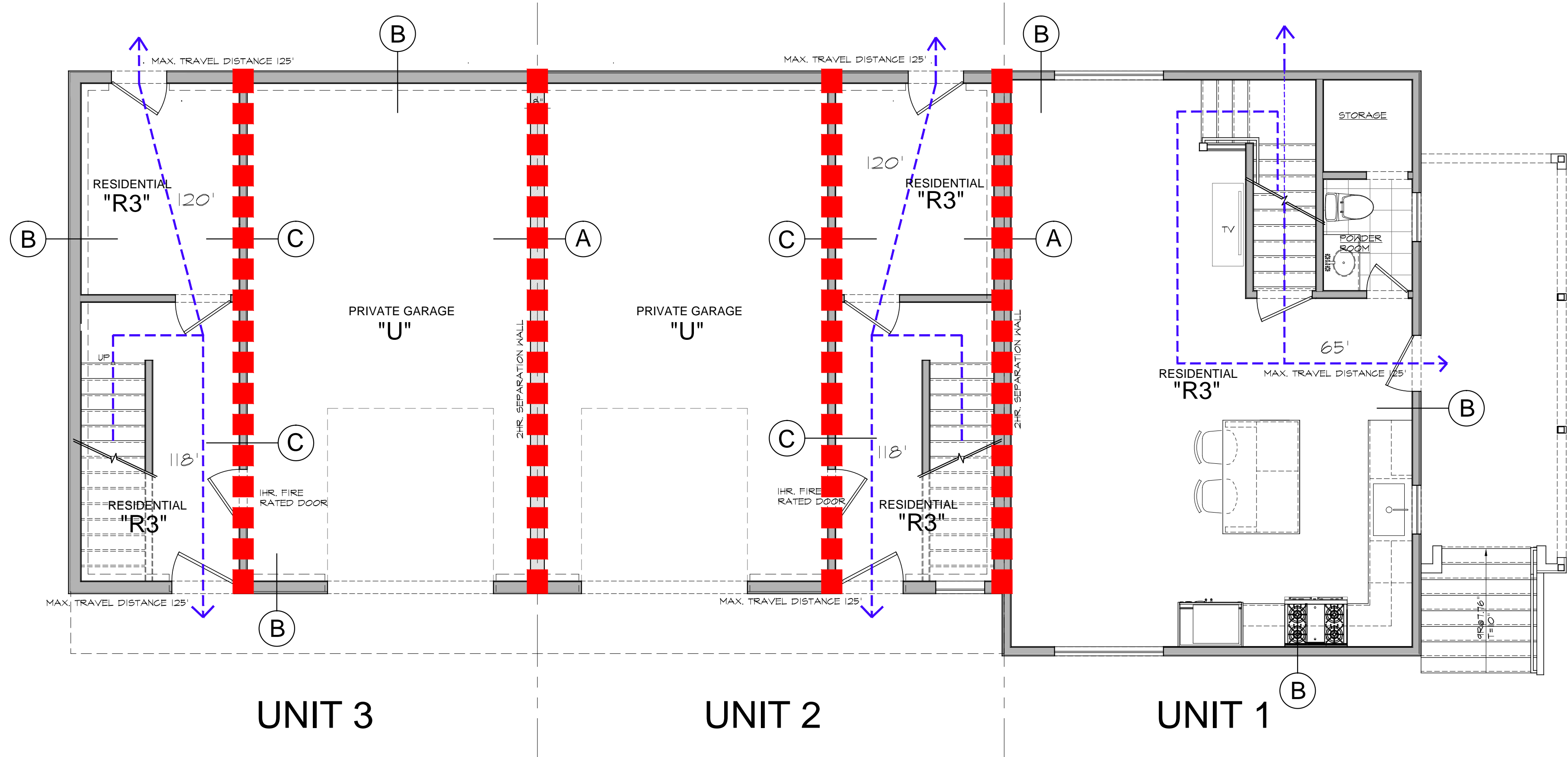
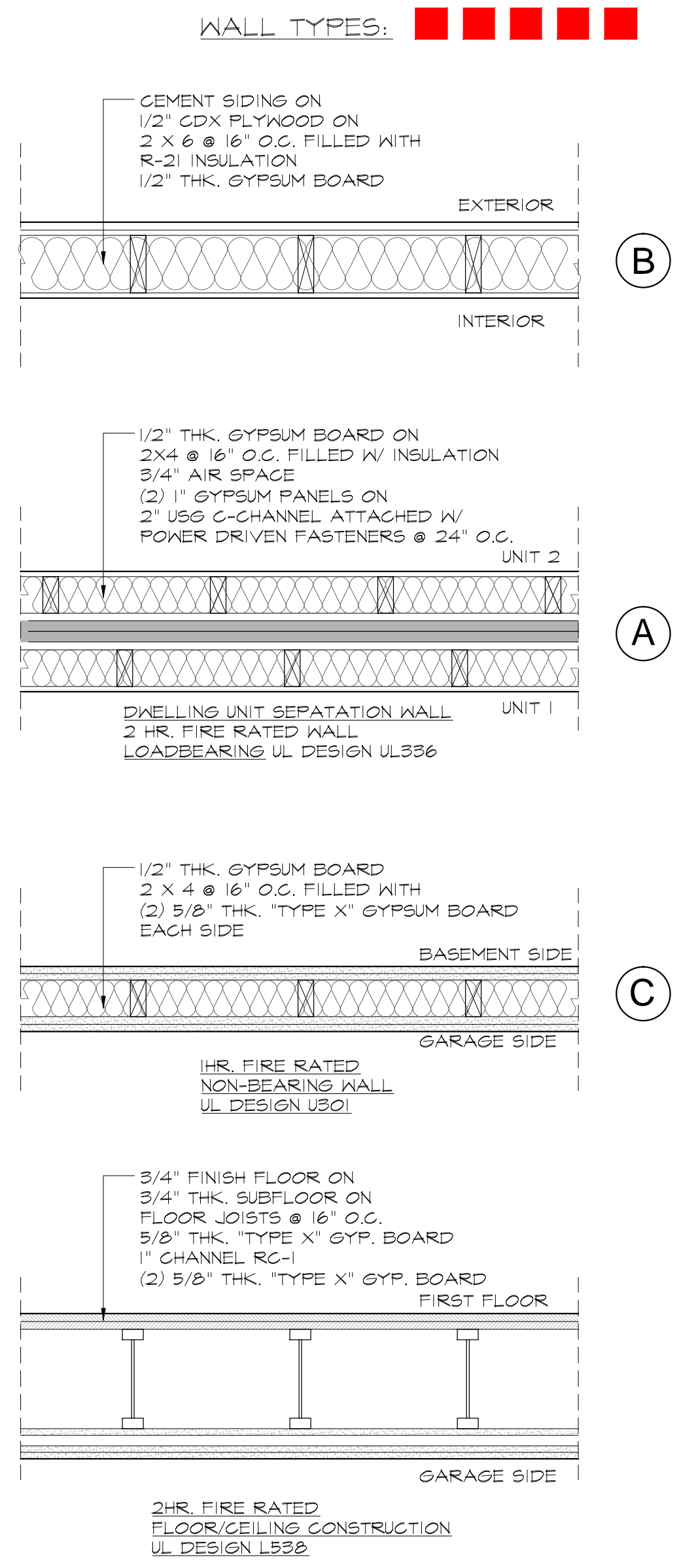
Dwg. Name:
**ELEVATIONS w/
COLOR SAMPLES**

1
A4.0
ELEVATION facing GARAGES
SCALE: 1/4"=1'-0"

1
A4.0
FRONT ELEVATION
SCALE: 1/4"=1'-0"

ZONING INFORMATION			ZONING DISTRICT "RM-F"	
REGULATION	REQUIRED	EXISTING	PROPOSED	HISTORIC DISTRICT BONUS
LOT AREA	5,000 SF	5,244 SF	NO CHANGE	NO CHANGE
STREET FRONTAGE	50 FT	45.67 FT	NO CHANGE	NO CHANGE
MAX. HEIGHT (STORIES)	4	1 1/2	4	5
MAX. HEIGHT (FEET)	40' MAX	17'-6" +/-	41'	50'
FRONT SETBACK	15/40' MIN	7.7"	NO CHANGE	11.25/30'
REAR SETBACK	30' MIN	57'	25.87'	22.5'
SIDE SETBACK	8/18' MIN	4.5/20.8'	5/21.3'	4/9'
BUILDING COVERAGE	30% OF LOT AREA 30% X 5244 = 1573 SF	1384 SF	700+630+630 = 1960 SF 1960/5244 = 37.4%	1573 + 25% = 1966 SF 1960/5244 = 37.5%
EXIST. BUILDING "UNIT 1" TOWNHOUSE "UNIT 2" TOWNHOUSE "UNIT 3"		1092 SF 0 SF 0 SF	700 SF 630 SF 630 SF	TOTAL 1960 SF MAX. 1966 SF > 1960 SF
PARKING SPACES	3	1	3	n/a

BUILDING AND FIRE INFORMATION		
APPLICABLE CODES:	2015 INTERNATIONAL BUILDING CODE 2018 CONNECTICUT STATE CODE 2015 INTERNATIONAL ENERGY CONSERVATION CODE 2015 INTERNATIONAL MECHANICAL CODE 2015 INTERNATIONAL PLUMBING CODE	
OCCUPANCY CLASSIFICATION:	R-3 TOWNHOUSE, 4 STORIES AND LESS THAN 60 FT IN HEIGHT UNIT No.1 TOTAL FLOOR AREA 700 SF UNIT No.2 TOTAL FLOOR AREA 630 SF UNIT No.3 TOTAL FLOOR AREA 630 SF	
U - PRIVATE GARAGE AT GRADE LEVEL		
CONSTRUCTION CLASSIFICATION (TABLE 601, 602):	5B DWELLING UNITS (R-3) 5B PRIVATE GARAGES (U) PRIMARY STRUCTURE FRAME OHR. BEARING WALLS EXTERIOR/INTERIOR OHR./HR. NON BEARING WALLS AND PARTITIONS OHR. FLOOR CONSTRUCTION OHR. ROOF CONSTRUCTION OHR.	
FIRE RESISTANCE RATINGS BASED ON FIRE SEPARATION FOR BUILDINGS NOT EQUIPPED WITH AUTOMATIC SPRINKLER SYSTEM (TABLE 508.4):		
EXTERIOR WALL, FIRE SEPARATION DISTANCE 5'-0" < X < 10'	OHR.	
SEPARATION WALL BETWEEN UNITS "R-3" AND "R-3"	2HR. UL DESIGN 336	
SEPARATION WALL BETWEEN GARAGE "U" AND "R-3"	2HR. UL DESIGN 301	
CEILING ASSEMBLY BETWEEN GARAGE "U" AND "R-3"	2HR. UL DESIGN 538	
OCCUPANT LOAD AND MEANS OF EGRESS:	ALLOWED: PROPOSED:	
TABLES 1004.1.2, 1003.1, 1015.3.2		
OCCUPANT LOAD (200 SF. GROSS)	200/2520 = 12	3
EGRESS WIDTH PER OCCUPANT:	0.3 / 0.2	36"
MAX. TRAVEL DISTANCE	125 FT.	120 FT.
NUMBER OF EXITS	2	2
MIN. EGRESS DOOR CLEAR WIDTH	32"	34"
MIN. EGRESS DOOR CLEAR HEIGHT	80"	80"
EGRESS WINDOW CLEAR OPENING	5.75F	5.75F
EGRESS WINDOW MIN. DIM.	20"W X 24"H	
EGRESS WINDOW MAX. DIM. FROM FLOOR	44"	LESS
SAFETY GLASS PER 2015 IBC, 2403 AND 2406		



RECONSTRUCTION OF EXISTING DWELLING "UNIT 1" AND PROPOSED TWO TOWNHOUSES "UNIT 2 and 3" FOR: DARIUSZ LESNIEWSKI

16 REMINGTON ST.
 STAMFORD CT 06902

General Notes:

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Dwg. Name: GENERAL NOTES CODE NOTES

Date: 1/25/2023

Scale: AS NOTED

Sheet: G1.0

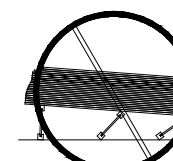
PERMIT SET

TJI INSTALLATION AND NAILING REQUIREMENTS



WARNING
Joists are unstable until braced laterally.

DO NOT allow workers to walk on joists until braced.



DO NOT stack building materials on unsheathed joists.

Bracing Includes:

- Blocking
- Sheathing
- Hangers
- Rim Board
- Stud Lines
- Rim Joist

WARNING NOTES:

- Lack of proper bracing during construction can result in serious accidents. Under normal conditions if the following guidelines are observed, accidents will be avoided.
- Install all blocking, hangers, rim boards and rim joists at TJI® joist end supports.
- Establish a permanent deck (sheathing), nailed to the first 4 feet of joists at the end of the bay or braced end wall.
- Safety stud lines of 1x4 (minimum) must be nailed to a braced end wall or sheathed area.
- Sheathing must be properly nailed to each TJI® joist before additional loads can be placed on the system.
- Ends of cantilevers require stud lines on both the top and bottom flanges.
- TJI® joist flanges must remain straight within a tolerance of 1/8" from true alignment.

TJI NAILING REQUIREMENTS

TJI® joists at bearings: Two 8d (2 1/2") box nails (1 each side), 1 1/2" minimum from end.

Blocking panels, rim joist or rim board to bearing plate: TJI® blocking panels or rim joist: 10d (3") box nails at 6" o.c. Trus Joist rim board: Toenail with 10d (3") box nails at 6" o.c. or 16d (3 1/2") box nails at 12" o.c.

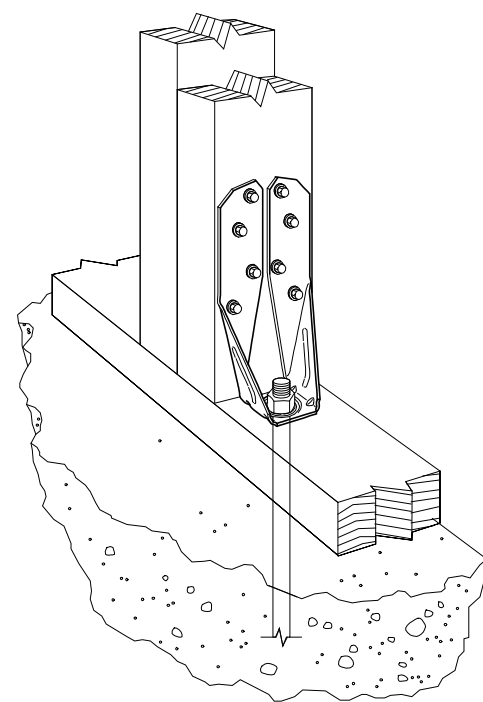
Shear transfer: Connections equivalent to decking nail schedule.

Rim board, rim joist or closure to TJI® joist: 1 1/2" width or less: Two 10d (3") box nails, one each at top and bottom flange.

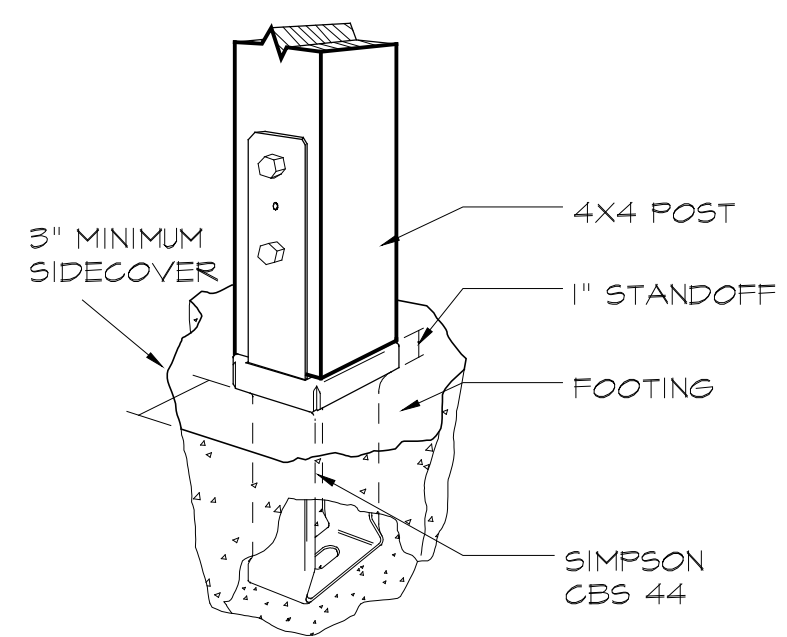
TJI® Pro™ 130, 350, 100C & 300C rim joist: Two 16d (3 1/2") box nails, one each at top and bottom flange.

TJI® Pro™ 550 & 400C rim joist: Toenail joist to rim joist with one 10d (3") box nail each side of joist top flange.

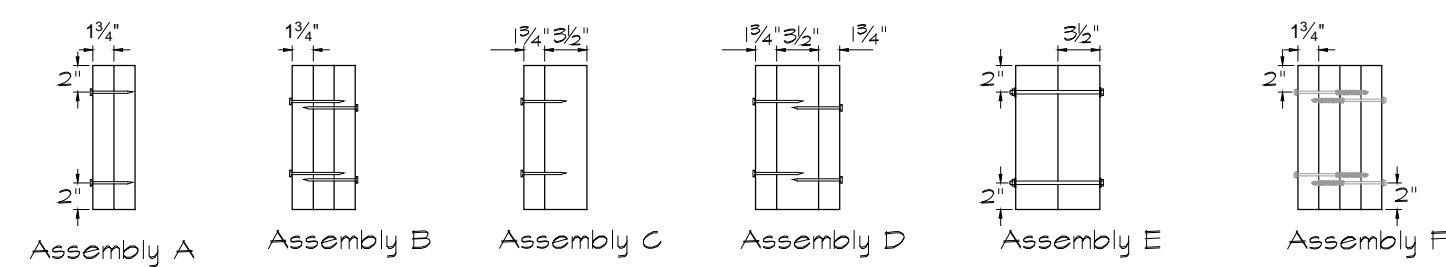
2x4 minimum squash blocks: Two 10d (3") box nails, one each at top and bottom flange.



5 DTT2Z CONNECTOR
S1.0 NO SCALE



4 POST @ FOOTING
S1.0 NO SCALE



Point Load Connector Spacing 8-Screw Connection
1/2" A307 through bolt or SDS, WS, Truss-LOK-EWP™ or SDW screw, typical

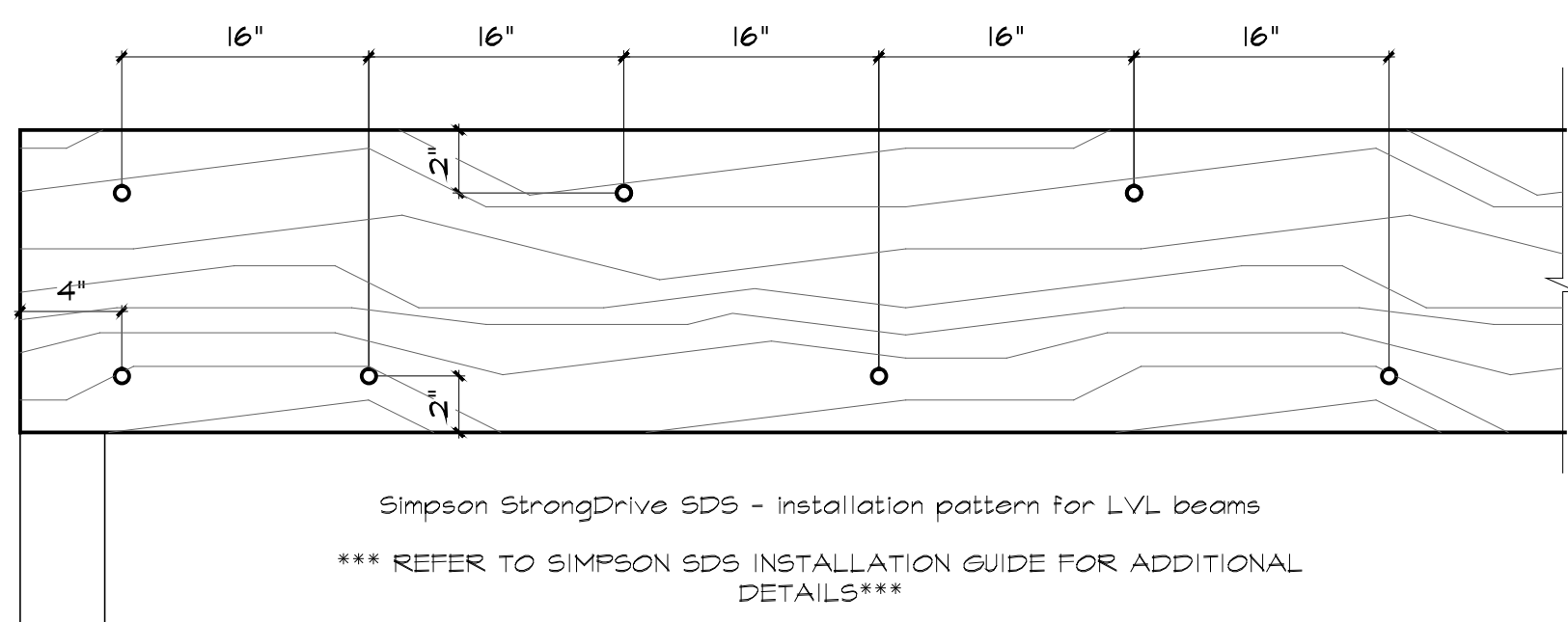
Point Load Connector Spacing 4- or 6-Screw Connection
1/2" A307 through bolt or SDS, WS, Truss-LOK-EWP™ or SDW screw, typical

Point Load Connector Spacing Nail Connection
10d (0.128" x 3") nails, typical. Stagger to prevent splitting. 2" spacing, typical. 1 1/2" minimum spacing, typical.

L20 spacing between staggered screws

L19 spacing between staggered screws

L21 There must be an equal number of nails on each side of the connection



3 NAILING PATTERN FOR STRUCTURAL MEMBERS
S1.0 NO SCALE

EXISTING CONDITIONS

ALL EXISTING CONDITIONS INDICATED ON THESE DRAWINGS, INCLUDING BUT NOT LIMITED TO SIZES AND LOCATIONS OF FOOTINGS, BEAMS, HEADERS, RAFTERS, JOISTS, TRUSSES, ETC. ARE BASED UPON INFORMATION PROVIDED BY THE CONTRACTOR AND/OR PROPERTY OWNER OR NONDESTRUCTIVE FIELD SURVEY. THESE EXISTING CONDITIONS MUST BE VERIFIED!

DURING CONSTRUCTION, SHOULD ANY DISCREPANCIES BETWEEN ACTUAL FIELD CONDITIONS AND THESE DRAWINGS BE DISCOVERED, IT IS THE CONTRACTOR'S AND/OR OWNER'S RESPONSIBILITY TO IMMEDIATELY NOTIFY THE FIELD INSPECTOR.

ROOF SHEATHING

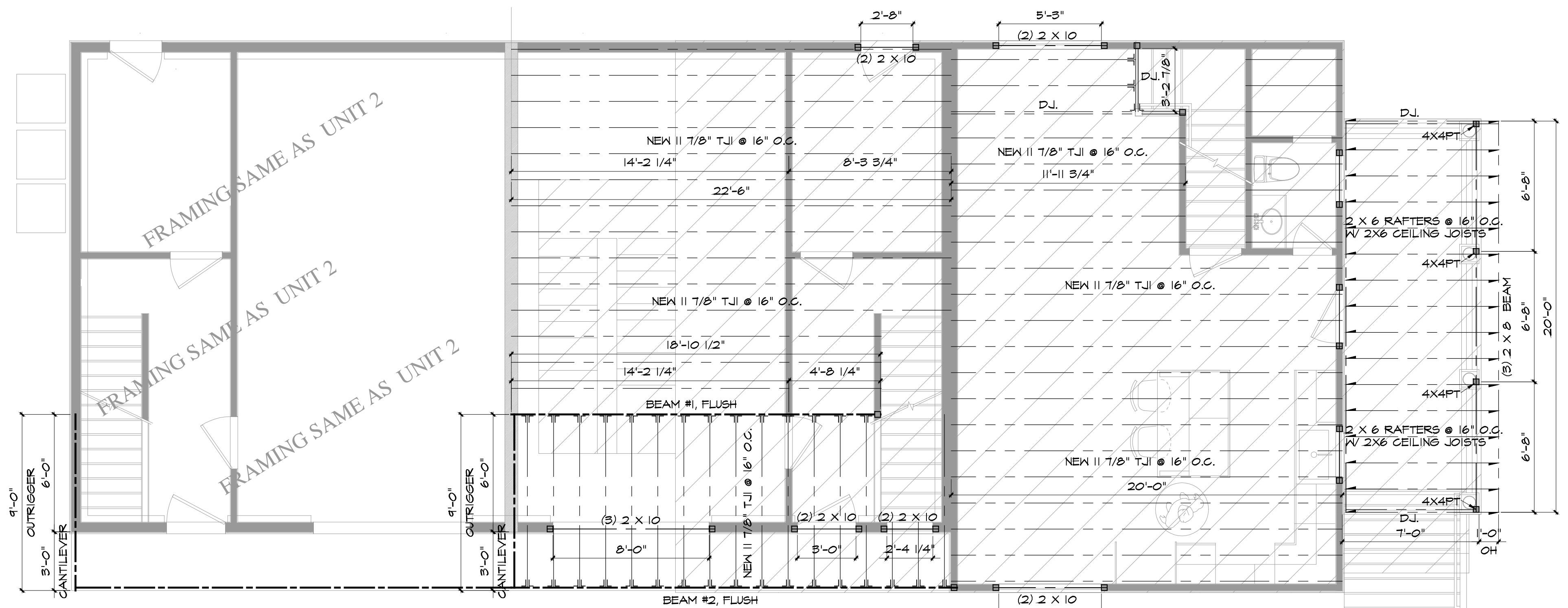
UNLESS OTHERWISE NOTED, ALL ROOF SHEATHING SHALL BE 1/2" (32/16) CDX PLYWOOD, NAILED WITH 10d COMMON NAILS AT 6" O.C. BOUNDARY, 6" O.C. ALL OTHER EDGES AND 12" O.C. FIELD.

FLOOR SHEATHING

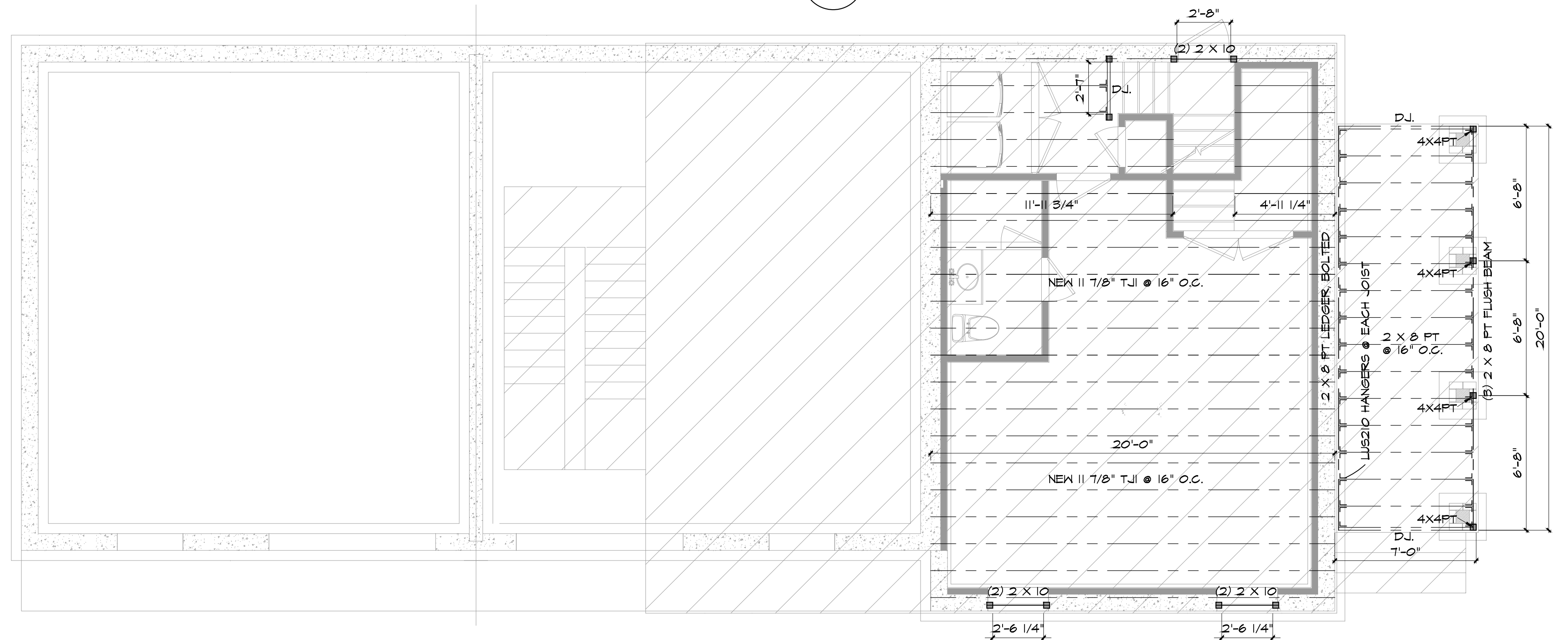
UNLESS OTHERWISE NOTED, ALL FLOOR SHEATHING SHALL BE 23/32" (48/24) T&G UNDERLAYMENT OR OSB, ATTACHED TO JOISTS WITH #10 SCREWS AT 6" & 10" O.C. ALL FLOOR ADHESIVE SHALL BE MANUFACTURED BY TOPS-PRO® OR "JAWS" NO SUBSTITUTE. INSTALL FLOOR SHEATHING SCREWS NO MORE THAN ONE HOUR AFTER APPLICATION OF THE ADHESIVE. UPON COMPLETION OF FLOOR FRAMING, INSTALL SCREWS THROUGH THE "DIAMOND HOLE" ON THE BOTTOM OF EACH ITT JOIST HANGER. INSTALL 6 SCREWS AT 6" ON CENTER THROUGH THE BOTTOM OF THE SHEATHING INTO THE BOTTOM PLATE AT EACH END OF ALL NONBEARING WALLS.

LEGEND:

_____	RIDGE BEAM:
_____	JOIST:
_____	RAFTER:
_____	BEAM:
_____	HEADER:
_____	BEARING WALL:
_____	POST:
_____	POST UP:
_____	STEEL COLUMN:



2 FIRST FLOOR FRAMING
S1.0 SCALE: 1/4"=1'-0"



1 BASEMENT FRAMING
S1.0 SCALE: 1/4"=1'-0"

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Dwg. Name:

FRAMING PLANS DETAILS

Sheet:

Date: 1/25/2023

Scale: AS NOTED

PERMIT SET

S1.0

LEGEND:

RIDGE BEAM: _____

JOIST: _____

RAFTER: _____

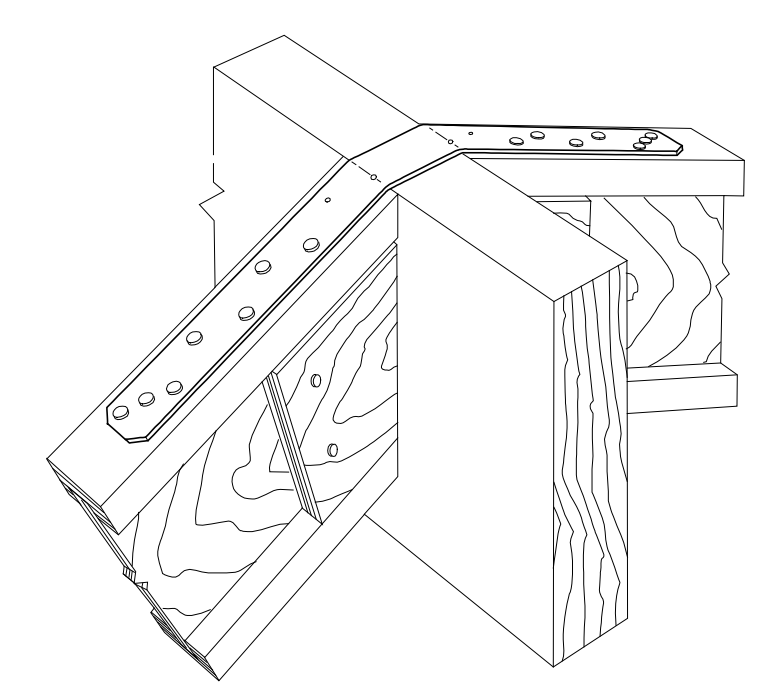
BEAM: _____

HEADER: _____

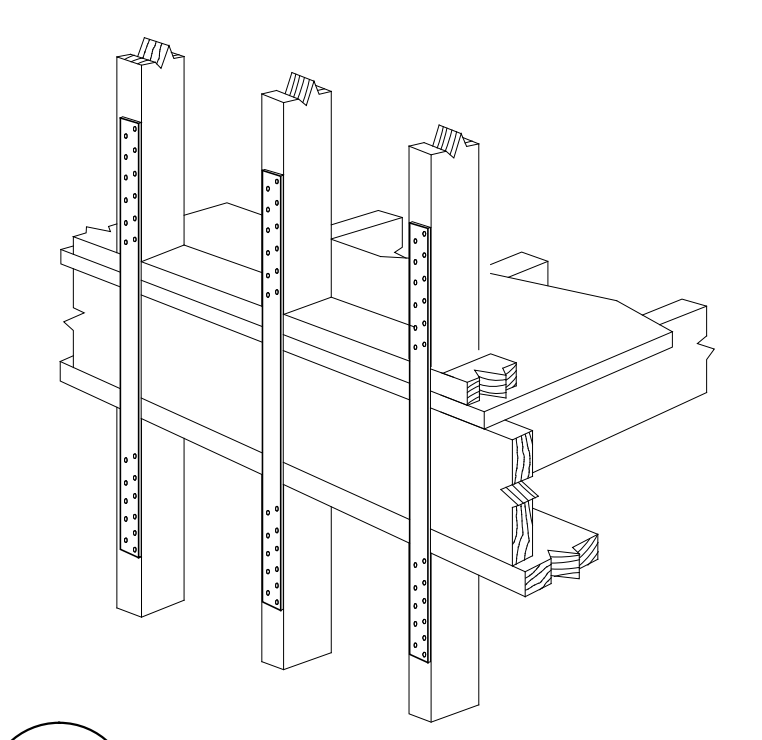
BEARING WALL: _____

POST: _____ POST UP:

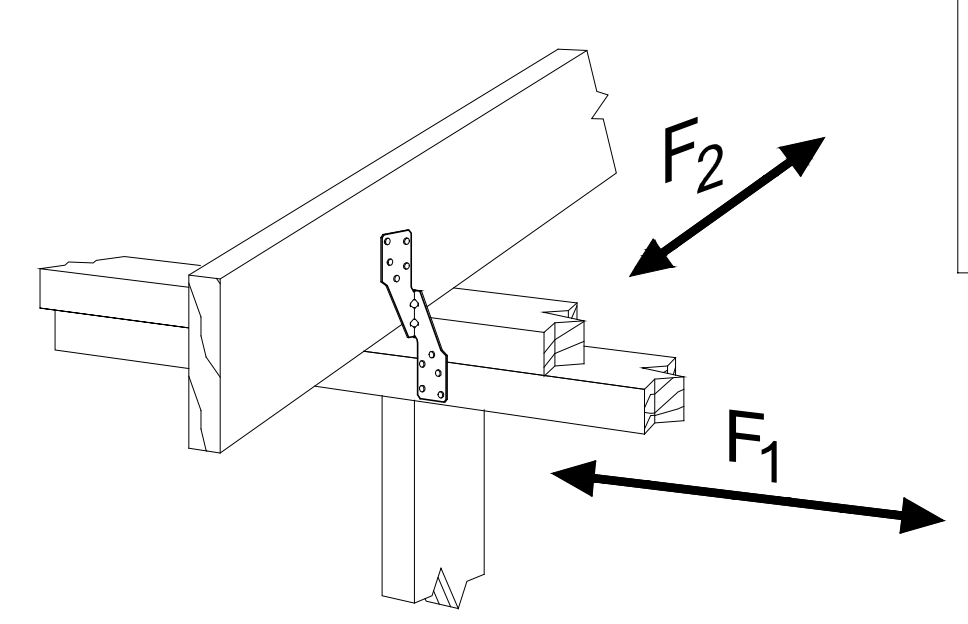
STEEL COLUMN: _____



4 LSTA RAFTER TO RAFTER
S1.1 NO SCALE



5 LSTA FLOOR TO FLOOR
S1.1 NO SCALE



6 H2.5 RAFTER TO PLATE
S1.1 NO SCALE

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ROOF SHEATHING

UNLESS OTHERWISE NOTED, ALL ROOF SHEATHING SHALL BE 1/2" (32/16) CDX PLYWOOD, NAILED WITH 10d COMMON NAILS AT 6" O.C. BOUNDARY, 6" O.C. ALL OTHER EDGES AND 12" O.C. FIELD.

FLOOR SHEATHING

UNLESS OTHERWISE NOTED, ALL FLOOR SHEATHING SHALL BE 23/32" (48/24) T&G UNDERLAYMENT OR OSB, ATTACHED TO JOISTS WITH #10 SCREWS AT 6" & 10" O.C. ALL FLOOR ADHESIVE SHALL BE MANUFACTURED BY "TOPS-PRO" OR "JAWS". NO SUBSTITUTE. INSTALL FLOOR SHEATHING SCREWS NO MORE THAN ONE HOUR AFTER APPLICATION OF THE ADHESIVE. UPON COMPLETION OF FLOOR FRAMING, INSTALL SCREWS THROUGH THE "DIAMOND HOLE" ON THE BOTTOM OF EACH ITT JOIST HANGER. INSTALL 6 SCREWS AT 6" ON CENTER THROUGH THE BOTTOM OF THE SHEATHING INTO THE BOTTOM PLATE AT EACH END OF ALL NONBEARING WALLS.

TJI INSTALLATION AND NAILING REQUIREMENTS

WARNING
Joists are unstable until braced laterally.

Bracing Includes:
• Blocking • Sheathing
• Hangers • Rim Board
• Strut Lines • Rim Joist

DO NOT allow workers to walk on joists until braced. DO NOT stack building materials on unshathed joists.

WARNING NOTES:

Lack of proper bracing during construction can result in serious accidents. Under normal conditions if the following guidelines are observed, accidents will be avoided.

1. Install all blocking, hangers, rim boards and rim joists at TJI joist end supports.
2. Establish a permanent deck (sheathing), nailed to the first 4 feet of joists at the end of the bay or braced end wall.
3. Safety strut lines of 1x4 (minimum) must be nailed to a braced end wall or sheathed area.
4. Sheathing must be properly nailed to each TJI joist before additional loads can be placed on the system.
5. Ends of cantilevers require strut lines on both the top and bottom flanges.
6. TJI joist flanges must remain straight within a tolerance of 1/2" from true alignment.

TJI NAILING REQUIREMENTS

TJI joists at bearings: Two 8d (2 1/2") box nails (1 each side), 1 1/2" minimum from end.

Blocking panels, rim joist or rim board to bearing plate: TJI blocking panels or rim joist: 10d (3") box nails at 6" o.c. Trus Joist rim board: Toenail with 10d (3") box nails at 6" o.c. or 16d (3 1/2") box nails at 12" o.c.

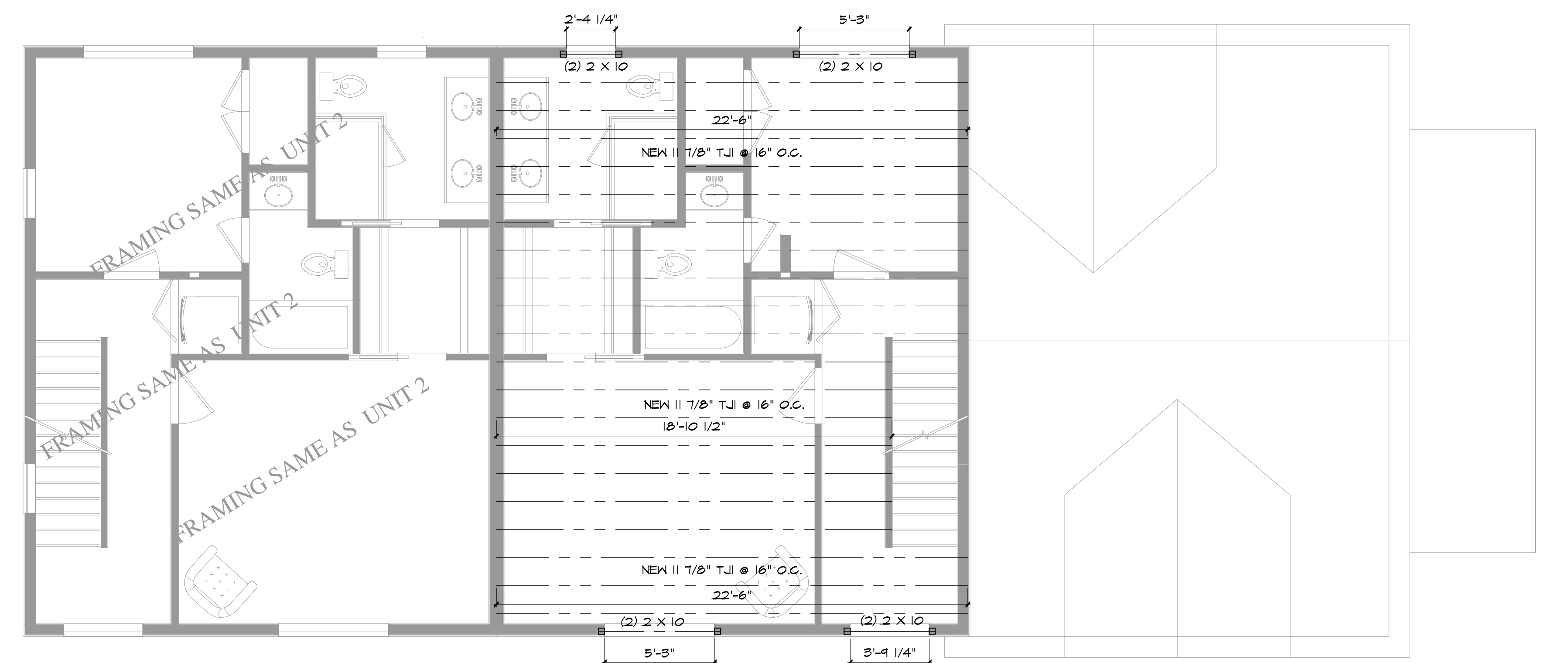
Shear transfer: Connections equivalent to decking nail schedule.

Rim board, rim joist or closure to TJI joist: 1 1/2" width or less: Two 10d (3") box nails, one each at top and bottom flange.

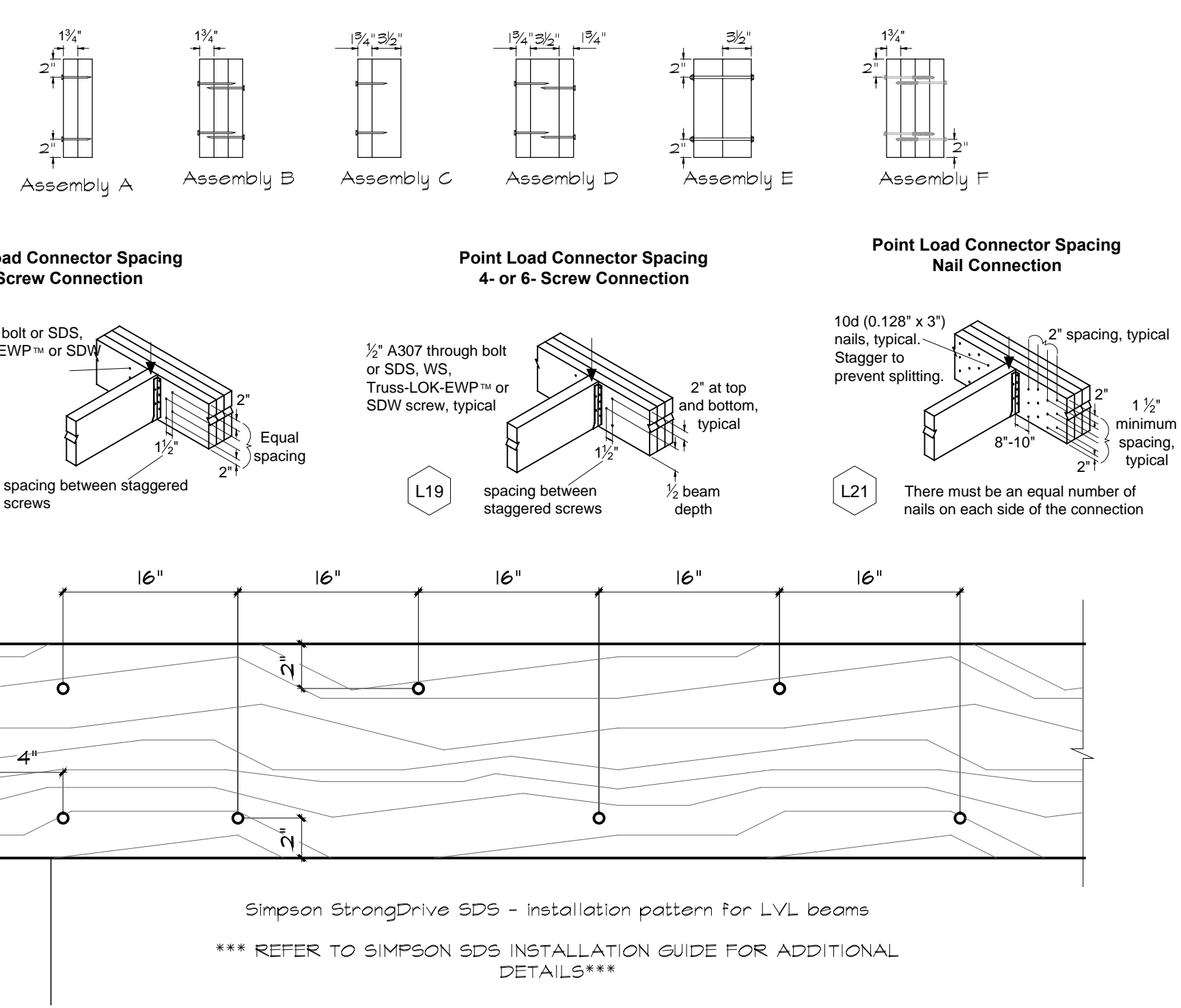
TJI Pro™ 130, 350, 100C & 300C rim joist: Two 16d (3 1/2") box nails, one each at top and bottom flange.

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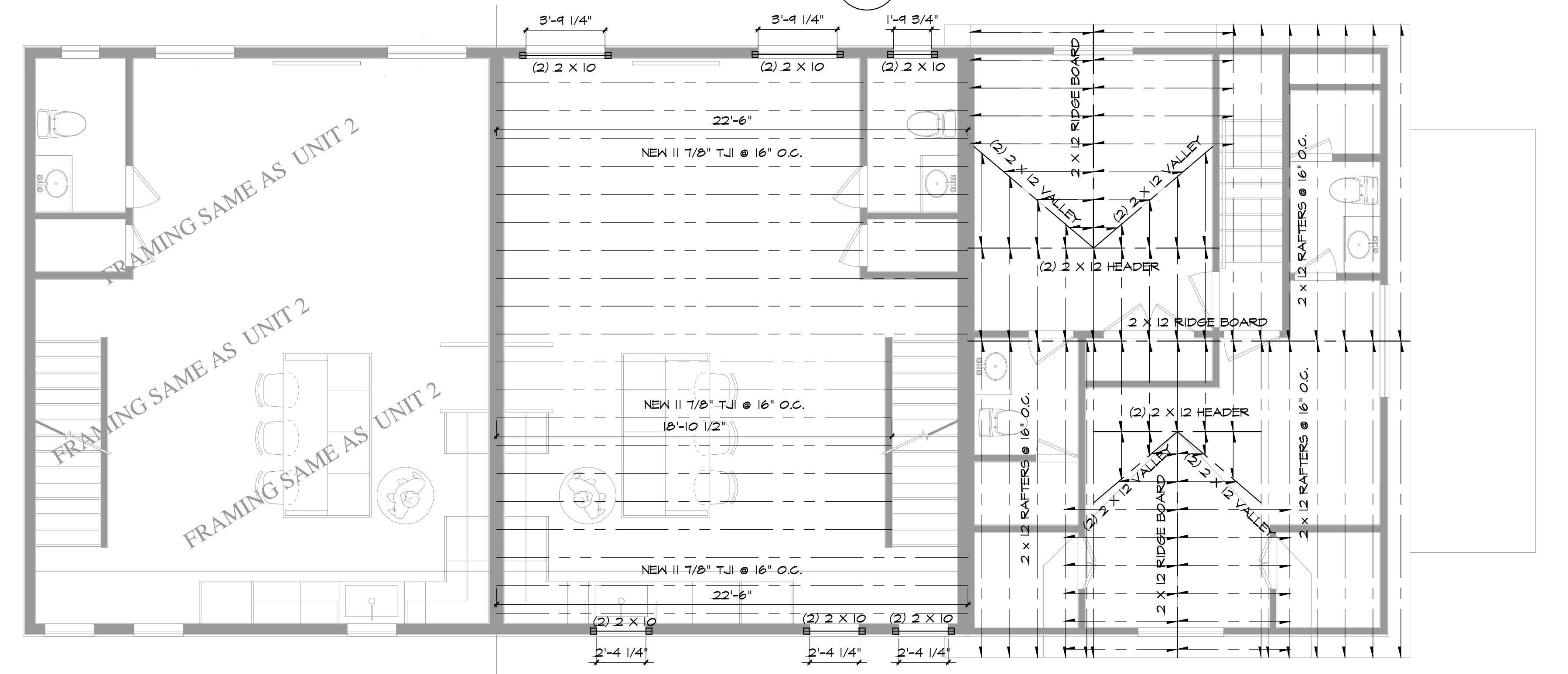
2x4 minimum squash blocks: Two 10d (3") box nails, one each at top and bottom flange.



2 THIRD FLOOR FRAMING
S1.1 SCALE: 1/4"=1'-0"



3 NAILING PATTERN FOR STRUCTURAL MEMBERS
S1.1 NO SCALE



1 SECOND FLOOR FRAMING
S1.1 SCALE: 1/4"=1'-0"

Revision:	Date:

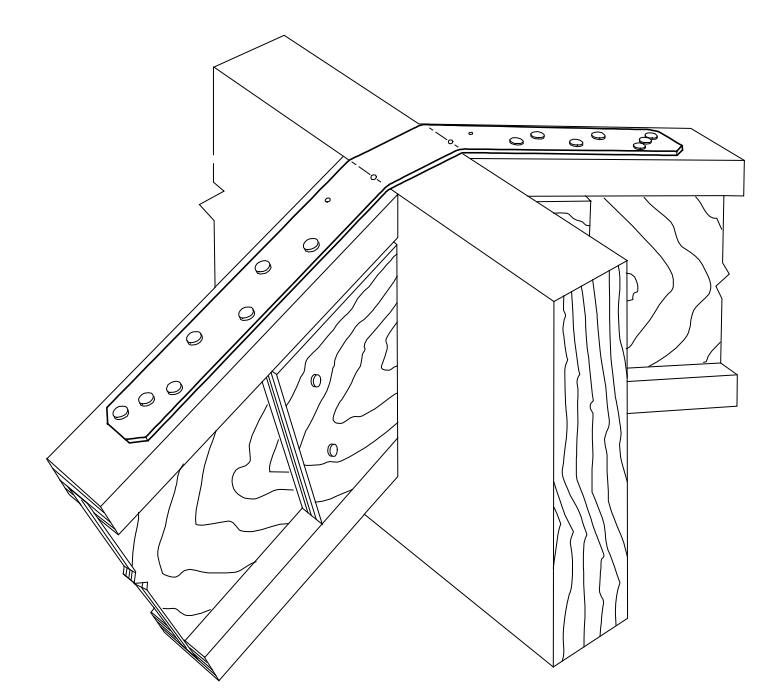
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Dwg. Name:
FRAMING PLANS DETAILS

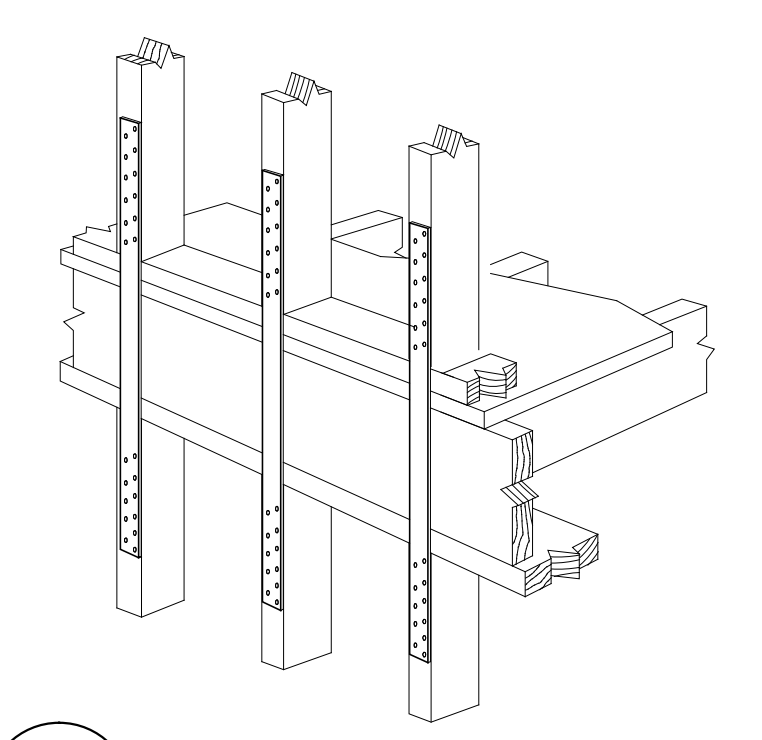
Date: 1/25/2023
Scale: AS NOTED
Sheet: **S1.1**

LEGEND:

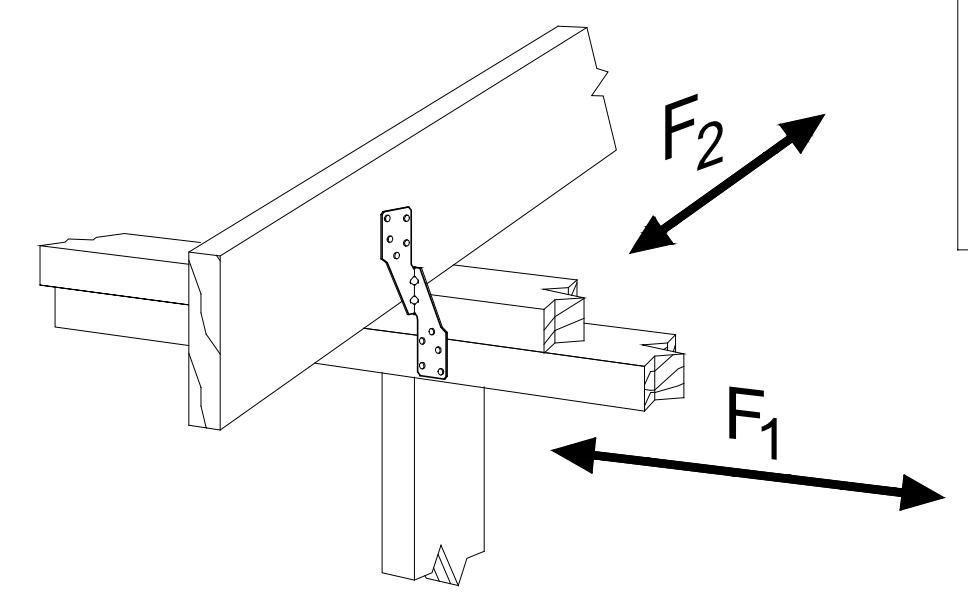
RIDGE BEAM: _____
 JOIST: _____
 RAFTER: _____
 BEAM: _____
 HEADER: _____
 BEARING WALL: _____
 POST: POST UP:
 STEEL COLUMN:



4 LSTA RAFTER TO RAFTER
S1.2 NO SCALE



5 LSTA FLOOR TO FLOOR
S1.2 NO SCALE



6 H2.5 RAFTER TO PLATE
S1.2 NO SCALE

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ROOF SHEATHING

UNLESS OTHERWISE NOTED, ALL ROOF SHEATHING SHALL BE 1/2" (3216) CDX PLYWOOD, NAILED WITH 10d COMMON NAILS AT 6" O.C. BOUNDARY, 6" O.C. ALL OTHER EDGES AND 12" O.C. FIELD.

FLOOR SHEATHING

UNLESS OTHERWISE NOTED, ALL FLOOR SHEATHING SHALL BE 23/32" (4824) T&G UNDERLAYMENT OR OSB, ATTACHED TO JOISTS WITH #10 SCREWS AT 6" & 10" O.C. ALL FLOOR ADHESIVE SHALL BE MANUFACTURED BY "TOPS-PRO" OR "JAWS". NO SUBSTITUTE. INSTALL FLOOR SHEATHING SCREWS NO MORE THAN ONE HOUR AFTER APPLICATION OF THE ADHESIVE. UPON COMPLETION OF FLOOR FRAMING, INSTALL SCREWS THROUGH THE "DIAMOND HOLE" ON THE BOTTOM OF EACH ITT JOIST HANGER. INSTALL 6 SCREWS AT 6" ON CENTER THROUGH THE BOTTOM OF THE SHEATHING INTO THE BOTTOM PLATE AT EACH END OF ALL NONBEARING WALLS.

TJI INSTALLATION AND NAILING REQUIREMENTS

WARNING
Joists are unstable until braced laterally.

Bracing Includes:
 • Blocking • Sheathing
 • Hangers • Rim Board
 • Strut Lines • Rim Joist

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WARNING NOTES:
Lack of proper bracing during construction can result in serious accidents. Under normal conditions if the following guidelines are observed, accidents will be avoided.

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6. TJI joist flanges must remain straight within a tolerance of 1/2" from true alignment.

TJI NAILING REQUIREMENTS

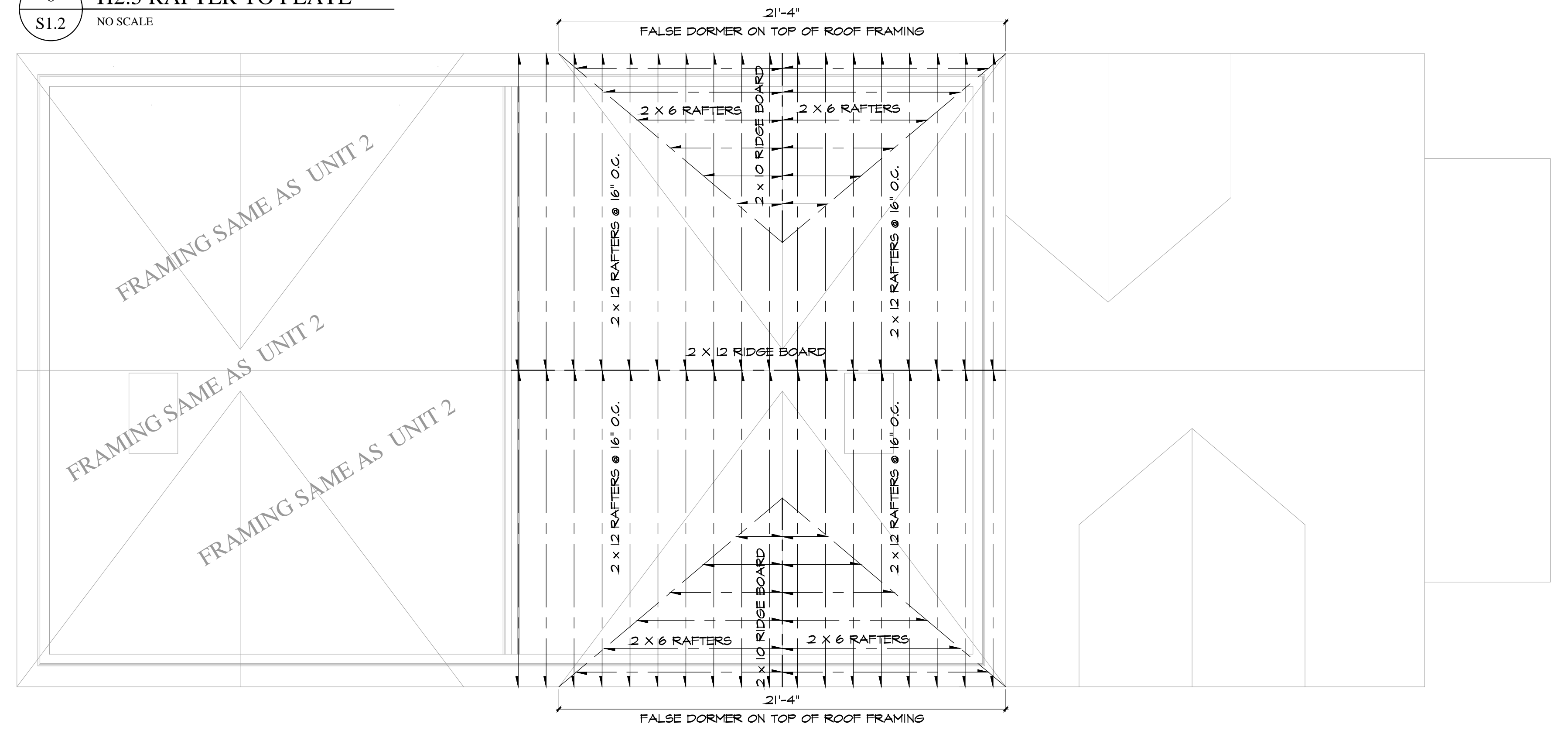
TJI joists at bearings: Two 8d (2 1/2") box nails (1 each side), 1 1/2" minimum from end.

Blocking panels, rim joist or rim board to bearing plate: TJI blocking panels or rim joist: 10d (3") box nails at 6" o.c. Truss Joist rim board: Toenail with 10d (3") box nails at 6" o.c. or 16d (3 1/2") box nails at 12" o.c.

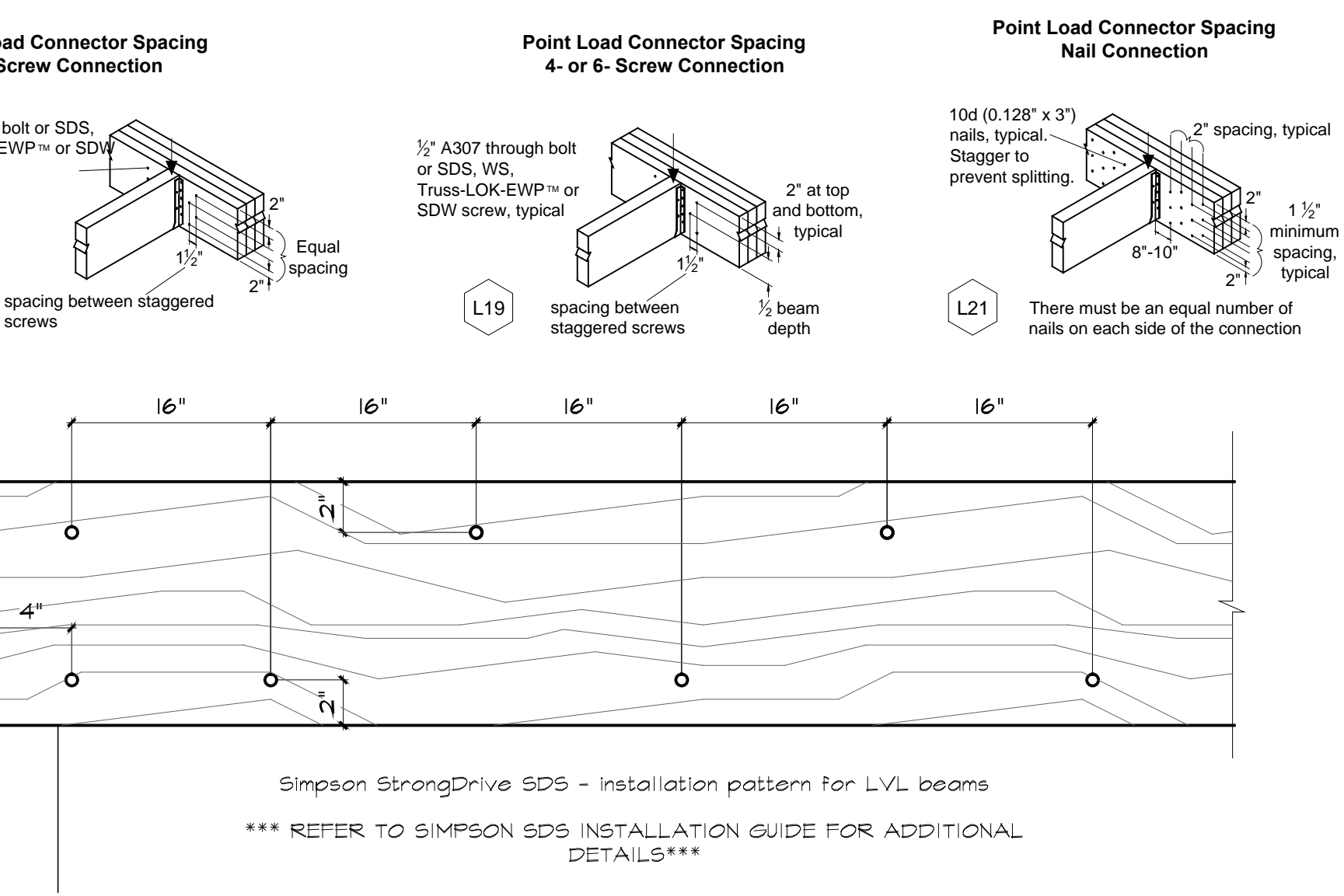
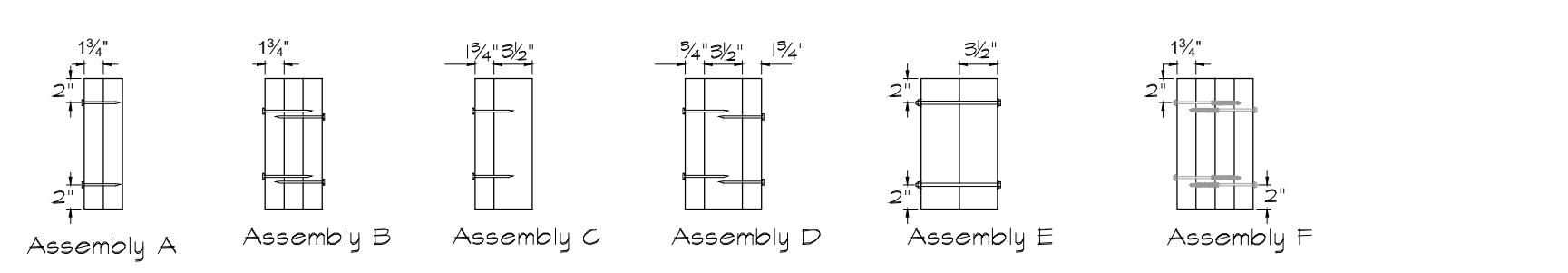
Shear transfer: Connections equivalent to decking nail schedule.

Rim board, rim joist or closure to TJI joist:
 1 1/2" width or less: Two 10d (3") box nails, one each at top and bottom flange.
 TJI Pro™ 130, 350, 100C & 300C rim joist: Two 16d (3 1/2") box nails, one each at top and bottom flange.
 TJI Pro™ 550 & 400C rim joist: Toenail joist to rim joist with one 10d (3") box nail each side of joist top flange.

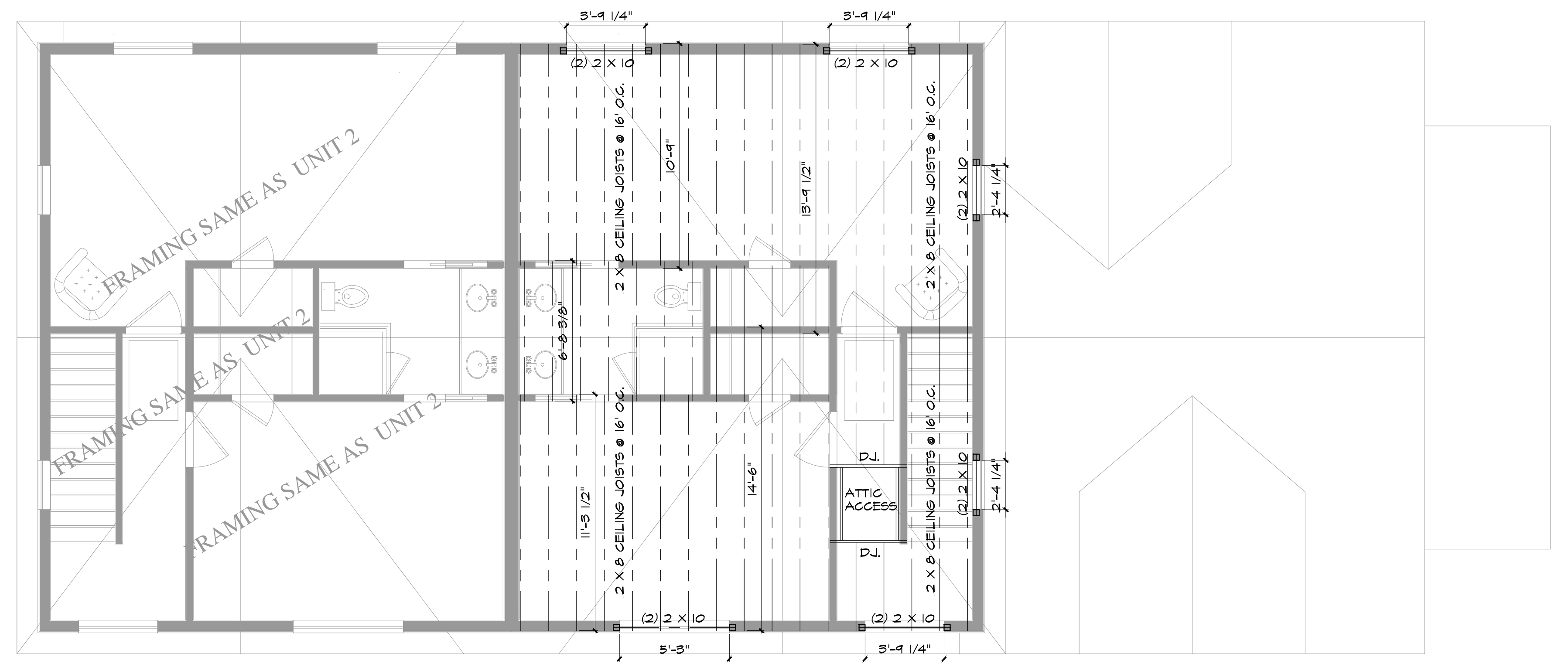
2x4 minimum squash blocks: Two 10d (3") box nails, one each at top and bottom flange.



2 ROOF FRAMING
S1.2 SCALE: 1/4"=1'-0"



3 NAILING PATTERN FOR STRUCTURAL MEMBERS
S1.2 NO SCALE



1 FORTH FLOOR FRAMING
S1.2 SCALE: 1/4"=1'-0"

Revision:	Date:

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Dwg. Name:
FRAMING PLANS DETAILS

Date:	1/25/2023	S1.2
Scale:	AS NOTED	